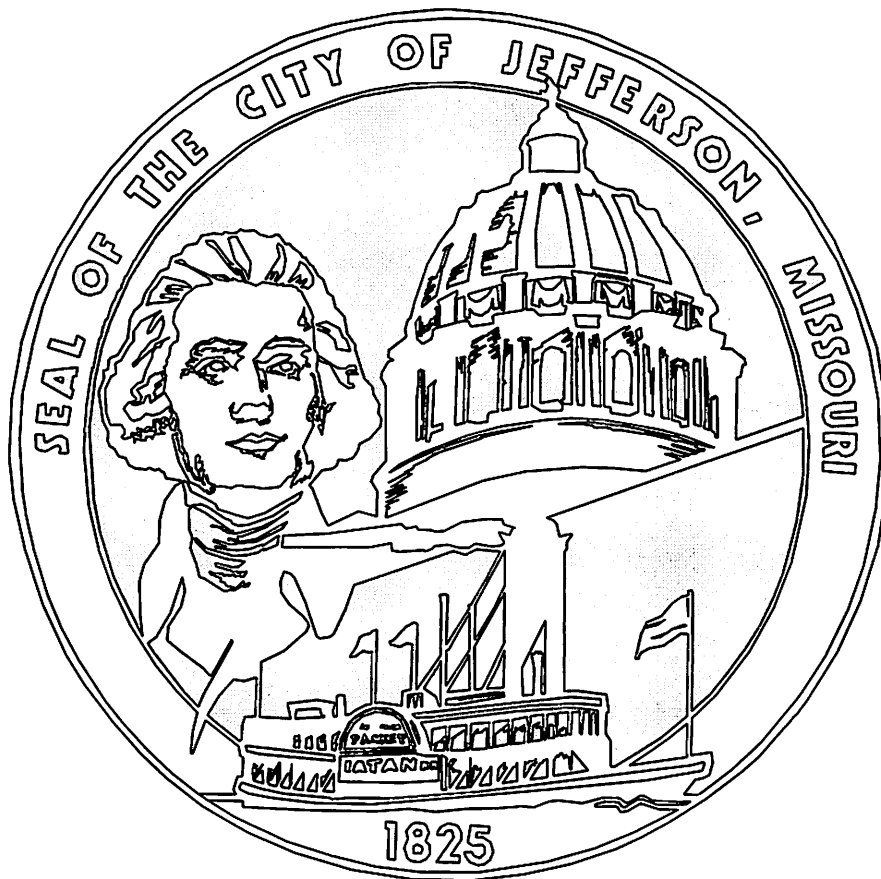




CITY OF JEFFERSON STANDARD DRAWINGS

REVISED : 1/2013
7TH EDITION



FOR FREE DOWNLOAD, GO TO CITY OF JEFFERSON WEB SITE:
<http://www.jeffcitymo.org/publicworks/engineering.html>

INDEX OF SHEETS

DIVISION II STREETS

DESCRIPTION:	SECTION NUMBER
<u>CITY OF JEFFERSON STREET DETAILS</u>	20.00
STANDARD STREET NOTES	20.01
STANDARD RESIDENTIAL & ARTERIAL STREET LAYOUTS ...	20.02
CONCRETE ROADWAY PAVEMENT JOINT LAYOUT	20.03
ROADWAY SECTION PAVEMENT JOINT LAYOUTS	20.04
OPTIONAL JOINT DETAILS	20.05
REINFORCEMENT FOR PCC PAVEMENT	20.06
TYPE "A" CURB & GUTTER DETAILS	20.07
STREET CUT REPAIR FOR PATCHING & BACKFILLING	
PAVED STREETS AND ALLEYS	20.08
 <u>SIDEWALKS</u>	 22.00
<u>SIDEWALK NOTES</u>	22.01
REINFORCEMENT DETAILS FOR SIDEWALK AT	
INLET LOCATIONS.....	22.02
TYPICAL SIDEWALK SECTIONS	22.03
CITY OF JEFFERSON DOWNTOWN TREE WELL &	
PROMENADE DETAILS	22.04
SIDEWALK TRENCH DRAIN	22.05
 A.D.A. <u>RAMP DETAILS</u>	 23.00
* A.D.A. <u>RAMP INFORMATION AND DETAILS</u>	23.01-23.08
 <u>RESIDENTIAL & COMMERCIAL DRIVES</u>	 24.00
<u>RESIDENTIAL DRIVE APPROACH DETAILS</u>	24.01
(FOR NEW CONSTRUCTION)	
RESIDENTIAL DRIVE APPROACH DETAILS	24.02
(FOR EXISTING CONCRETE, OR ASPHALT ROADWAYS	
WITH EXISTING CONCRETE CURB & GUTTER)	
RESIDENTIAL DRIVE APPROACH DETAILS.....	24.03
(FOR EXISTING MONOLITHIC CONCRETE ROADWAYS)	
RESIDENTIAL DRIVE APPROACH DETAILS.....	24.04
(AT LOCATIONS IN WHICH EXISTING SIDEWALK	
IS BEHIND THE CURB)	
RESIDENTIAL DRIVE APPROACH DETAILS.....	24.05
(LOCATIONS IN WHICH EXISTING SIDEWALK	
IS BEHIND CURB)	
RESIDENTIAL DRIVE SECTIONS	24.06
DRIVEWAY PROFILES FOR MAXIMUM DOWNGRADE	
OR UPGRADE VERTICAL CURVES	24.07
 <u>COMMERCIAL DRIVE APPROACHES</u>	 25.00
<u>COMMERCIAL DRIVE APPROACH DETAILS</u>	25.01
(FOR NEW CONSTRUCTION)	
COMMERCIAL DRIVE APPROACH DETAILS	25.02
(FOR EXISTING CONCRETE AND ASPHALT ROADWAYS)	
COMMERCIAL DRIVE SECTION	25.03

* Denotes Modification/Addition for year 2013
City of Jefferson Standards

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For comments or corrections please E-Mail at: jceengineering@jeffcitymo.org

 **Jefferson**
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION
320 E. McCarty Street
Jefferson City, Missouri, 65101
Ph:(573)634-6410
Fax:(573)634-6562
CITY OF JEFFERSON STANDARD DRAWINGS
REVISED EDITION: 1/2013

STANDARD STREET NOTES:

1. THE DEPARTMENT OF PUBLIC WORKS MUST BE NOTIFIED AT LEAST 24 HOURS PRIOR TO BEGINNING CONSTRUCTION, INCLUDING GRADING. NO STREET OR ANY PART THEREOF WILL BE ACCEPTED BY THE CITY OF JEFFERSON UNLESS PRIOR NOTICE HAS BEEN GIVEN TO THE DEPARTMENT OF PUBLIC WORKS.
2. ALL CONSTRUCTION INDICATED HEREON SHALL BE DONE IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS OF THE CITY OF JEFFERSON.
3. ALL DISTURBED RIGHT-OF-WAY SHALL BE SODDED OR SEEDED AND MULCHED BEFORE THE STREET WILL BE ACCEPTED BY THE CITY.

REVISION AND DESCRIPTION

DATE:

NO:

STANDARD STREET
NOTES

STANDARD DETAILS

CHECKED BY:

DRAWN BY:

DATE:

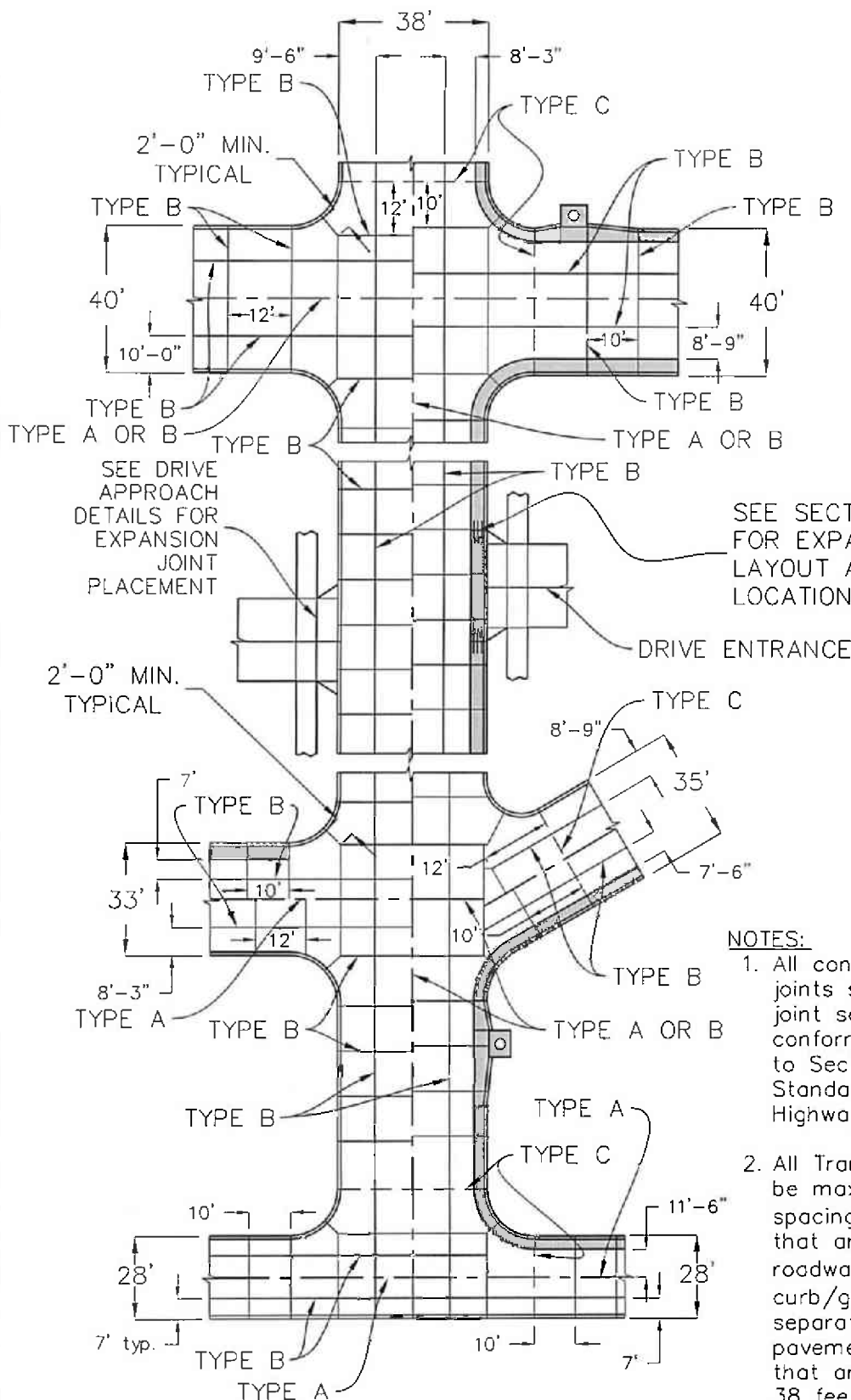


SHEET NUMBER

1 OF 8

SECTION

20.01



INTEGRAL ROADWAY/CURB PAVEMENT JOINT LAYOUT

NOTE:

Varying longitudinal joint patterns may be used upon approval by the Engineer.

NOTES:

1. All control and sawed joints shall be filled with joint sealer. Sealer shall conform to Section 1057.5, Missouri Standard Specifications for Highway Construction.
2. All Transverse joints shall be maximum of 10 feet spacing's for roadways that are 28 feet wide, and roadway's that the curb/gutter is poured separately from roadway pavement for roadways that are 33 feet, 35 feet, 38 feet, and 40 feet wide.
3. All Transverse joints shall be maximum of 12 feet spacing's for roadways that are poured monolithically with the curb/gutter and are 33 feet, 35 feet, 38 feet, and 40 feet wide.

NO:	DATE:	REVISION AND DESCRIPTION

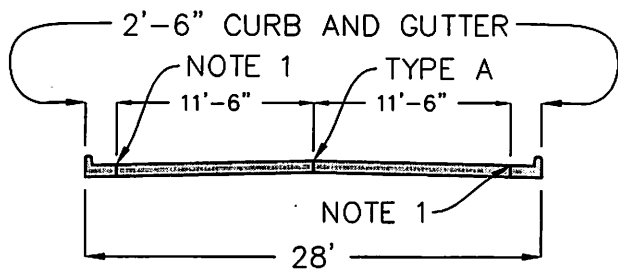
ROADWAY PAVEMENT JOINT LAYOUT	STANDARD DETAILS

CHECKED BY:	DRAWN BY:	DATE:

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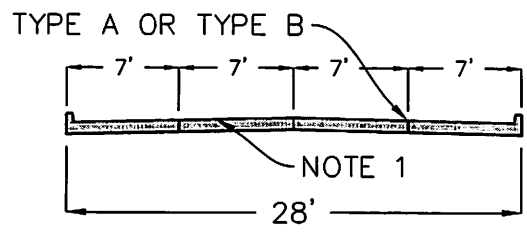
City of Jefferson
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING DIVISION

SHEET NUMBER
3 OF 8
SECTION
20.03



JOINT LAYOUT WITH CURB AND GUTTER
POURED SEPERATELY

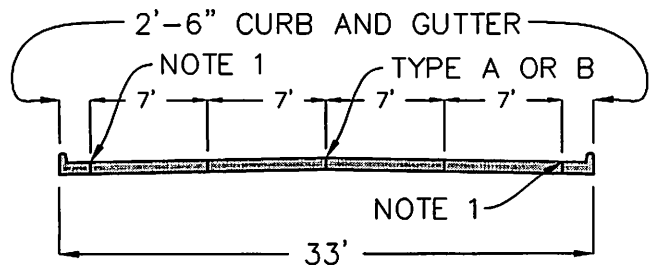
Transverse joint
spacing shall be
10 ft. max.



JOINT LAYOUT WITH CURB AND GUTTER
INTEGRAL TO ROADWAY

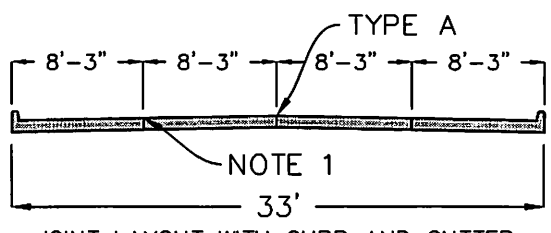
Transverse joint
spacing shall be
10 ft. max.

28 FT. ROADWAY SECTION WIDTH



JOINT LAYOUT WITH CURB AND GUTTER
POURED SEPERATELY

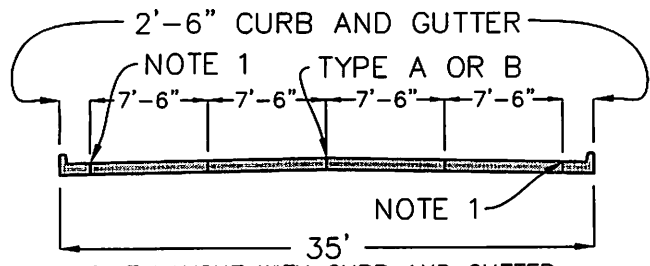
Transverse joint
spacing shall be
10 ft. max.



JOINT LAYOUT WITH CURB AND GUTTER
INTEGRAL TO ROADWAY

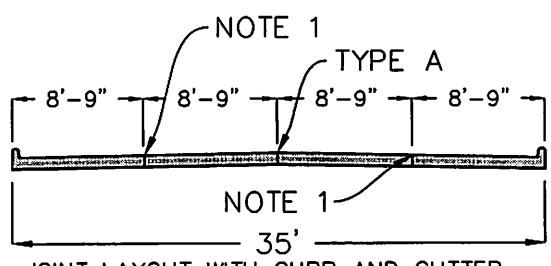
Transverse joint spacing
shall be 12 ft. max.

33 FT. ROADWAY SECTION WIDTH



JOINT LAYOUT WITH CURB AND GUTTER
POURED SEPERATELY

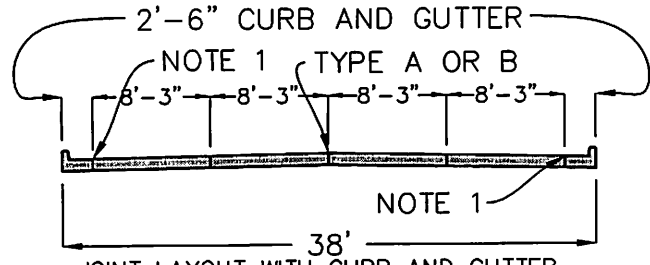
Transverse joint
spacing shall be
10 ft. max.



JOINT LAYOUT WITH CURB AND GUTTER
INTEGRAL TO ROADWAY

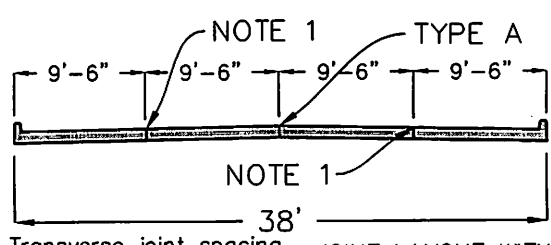
Transverse joint spacing
shall be 12 ft. max.

35 FT. ROADWAY SECTION WIDTH



JOINT LAYOUT WITH CURB AND GUTTER
POURED SEPERATELY

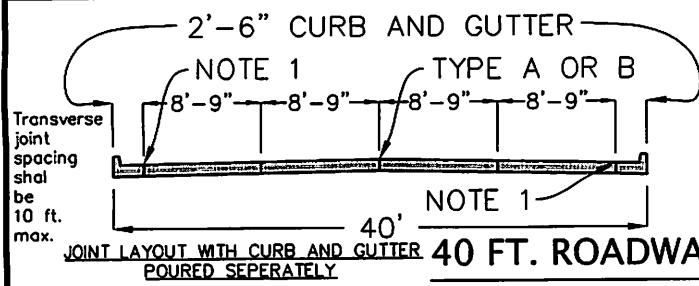
Transverse joint
spacing shall be
10 ft. max.



JOINT LAYOUT WITH
CURB AND GUTTER
INTEGRAL TO ROADWAY

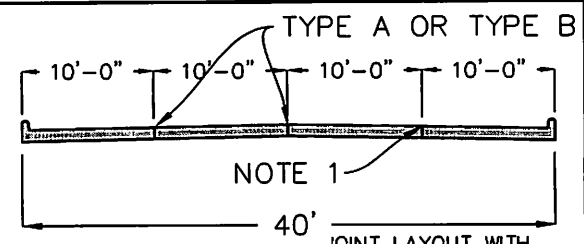
Transverse joint spacing
shall be 12 ft. max.

38 FT. ROADWAY SECTION WIDTH



JOINT LAYOUT WITH CURB AND GUTTER
POURED SEPERATELY

Transverse
joint
spacing
shal
be
10 ft.
max.




JOINT LAYOUT WITH
CURB AND GUTTER
INTEGRAL TO ROADWAY

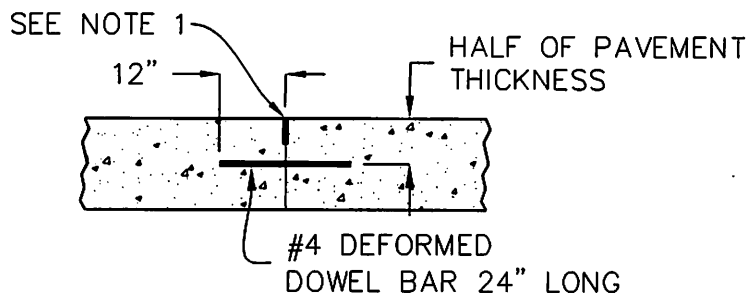
Transverse joint spacing
shall be 12 ft. max.

40 FT. ROADWAY SECTION WIDTH

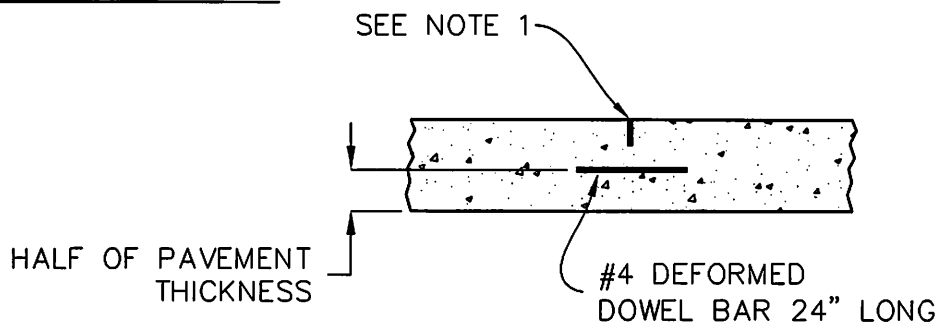
NOTES:

1. All control and sawed joints shall be filled with joint sealer. Sealer shall conform to Section 1057.5, Missouri Standard Specifications for Highway Construction.

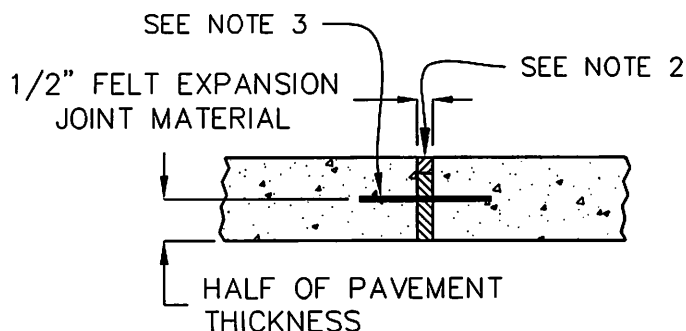
ROADWAY SECTION PAVEMENT JOINT LAYOUTS		NO:	DATE:	REVISION AND DESCRIPTION
STANDARD DETAILS				
CHECKED BY:				
DRAWN BY:				
DATE:				
20.01 DWG				
<div>City of  Jefferson DEPARTMENT OF PUBLIC WORKS ENGINEERING DIVISION</div>				
20.01 DWG				
SHEET NUMBER				
4 OF 8				
SECTION				
20.04				



TYPE A
(CONSTRUCTION JOINT)




TYPE B
(CONTROL JOINT)

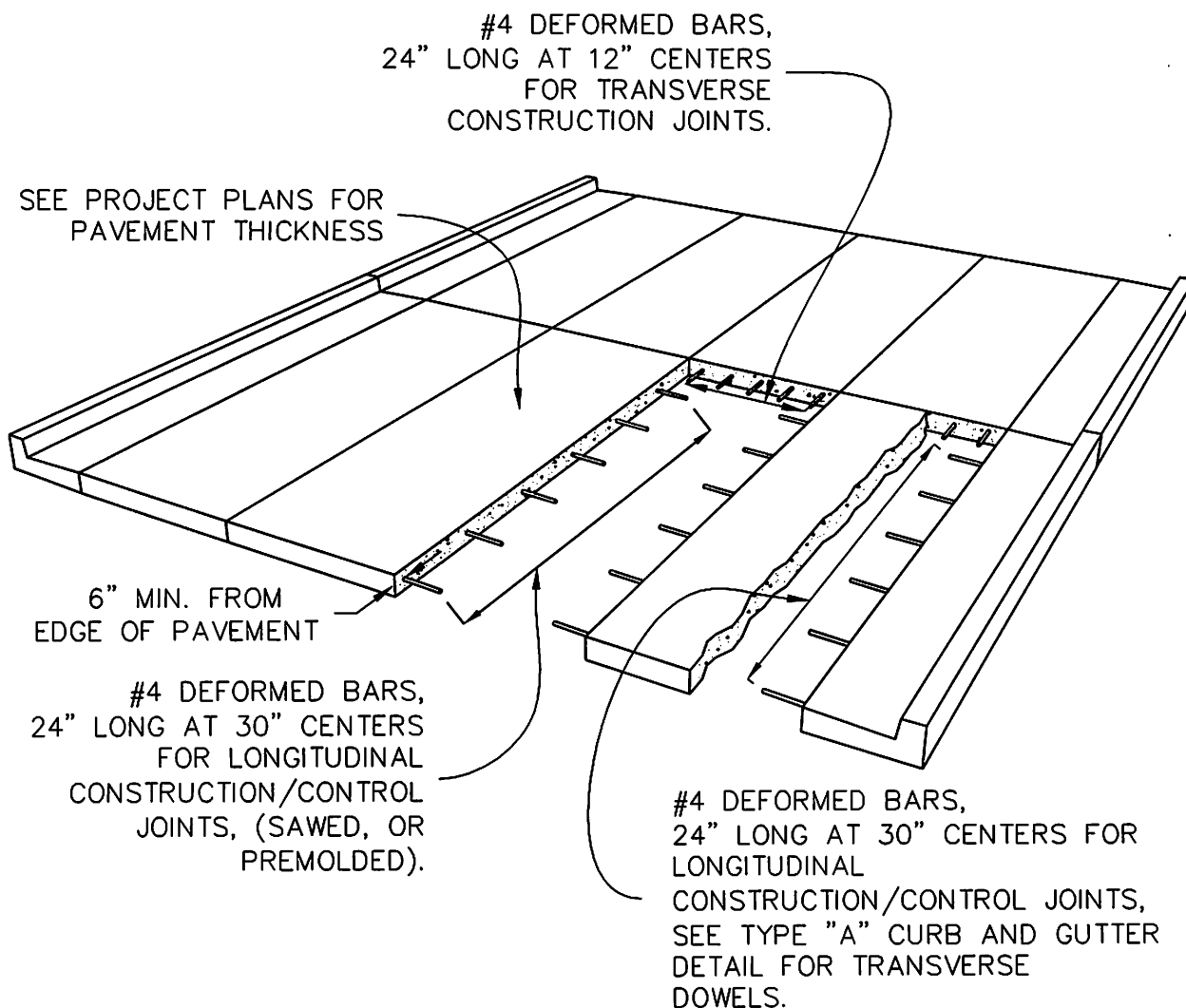


TYPE C
(EXPANSION JOINT DETAIL)

NOTES

1. CENTERLINE OF SAWED JOINT 3/16" MIN. WIDTH x 1/4 OF PAVEMENT THICKNESS.
2. REMOVE TOP 1/4 OF PAVEMENT THICKNESS OF THE EXPANSION JOINT MATERIAL AND FILL WITH JOINT SEALER THAT CONFORMS TO SECTION 1057.5 MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.
3. 5/8"Ø x 18" LONG SMOOTH DOWEL BARS. THE DOWEL BARS SHALL BE GREASED AND WRAPPED ON ONE END WITH EXPANSION TUBES.
4. ALL CONTROL AND SAWED JOINTS SHALL BE FILLED WITH SEALER. SEALER SHALL CONFORM TO SECTION 1057.5 MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

SHEET NUMBER 5 OF 8 SECTION 20.05				CHECKED BY:	JOINT DETAILS		NO:	DATE:	REVISION AND DESCRIPTION
							1	1/2010	*** ADDED EXPANSION JOINT DETAIL
					DRAWN BY:	STANDARD DETAILS			
					DATE:				

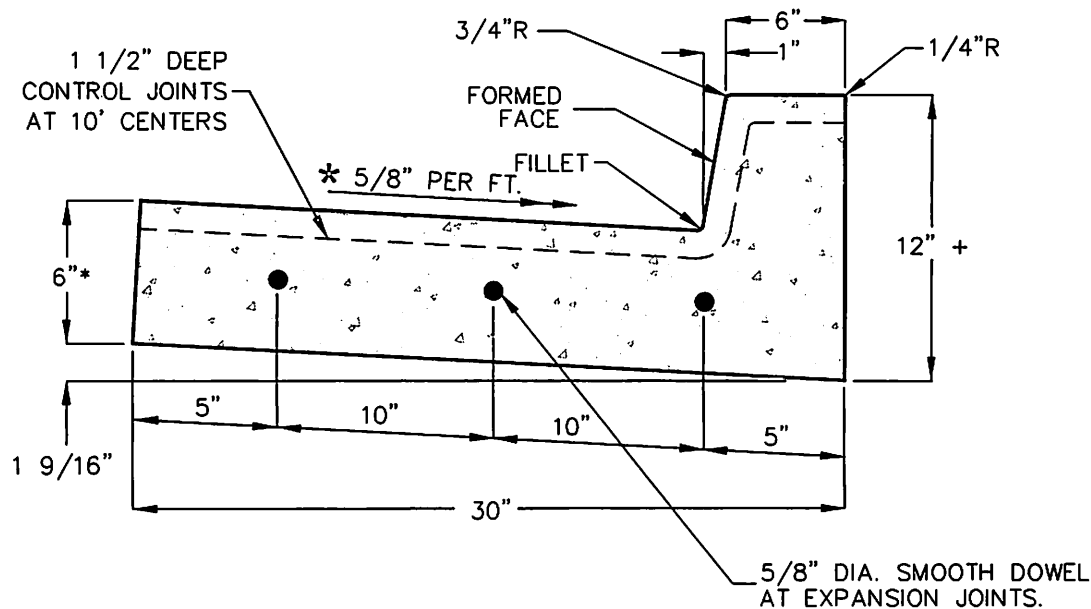


REINFORCEMENT FOR PCC PAVEMENT

NOTES:

1. EXPANSION JOINTS SHALL BE PLACED IN CURB AND GUTTER AT CURB RETURNS, INLET RETURNS, AND DRIVE APPROACHES. 3/4 INCH EXPANSION MATERIAL WITH 5/8"x24" SMOOTH DOWELS SHALL BE PLACED AT RADIUS POINTS. THESE DOWELS SHALL BE GREASED AND WRAPPED ON ONE END WITH EXPANSION TUBES.
2. REINFORCED JOINTS IN PCC PAVEMENT AS FOLLOWS:
 - A. TRANSVERSE SAWED JOINTS - NOT TIED.
 - B. TRANSVERSE CONSTRUCTION JOINTS - TIE WITH 1/2"x24" DEFORMED BARS AT 12 INCH CENTERS.
 - C. LONGITUDINAL CONSTRUCTION OF SAWED JOINTS - TIE WITH 1/2"x24" DEFORMED BARS AT 30 INCH CENTERS.
3. INSTALL DOWEL BARS AS SHOWN ON THE PLANS, OR A PARTICULAR DETAIL, OR AS DIRECTED BY THE ENGINEER.

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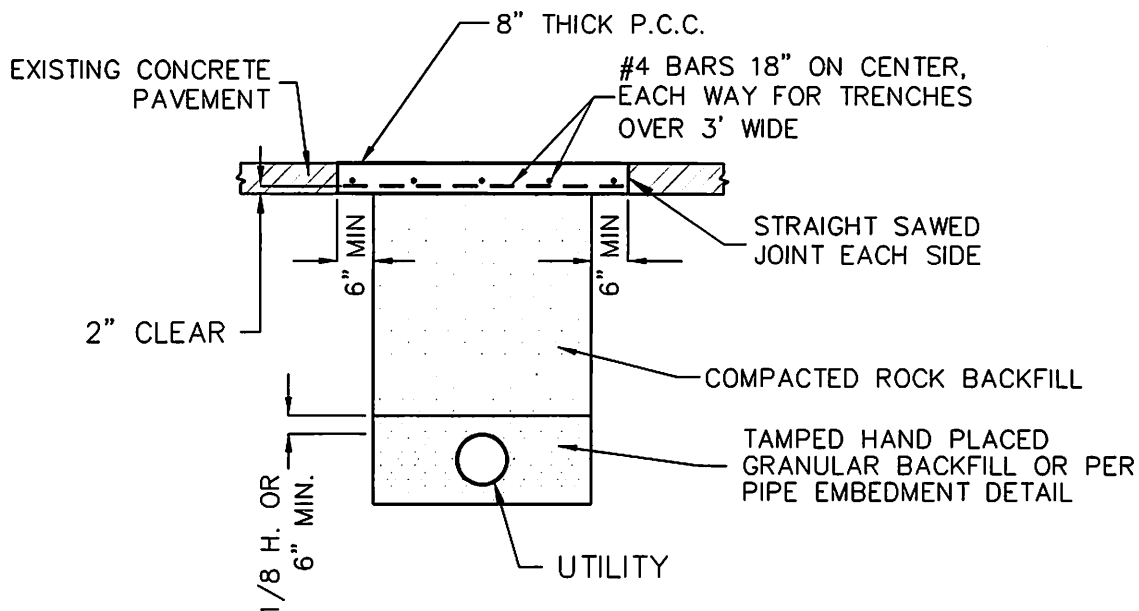
* 6" MIN.— MATCH PAVEMENT
THICKNESS OF P.C.C. PAVEMENT.
** 12" MIN.— INCREASE BY SAME
AMOUNT GUTTER PAN INCREASES.

NOTES:

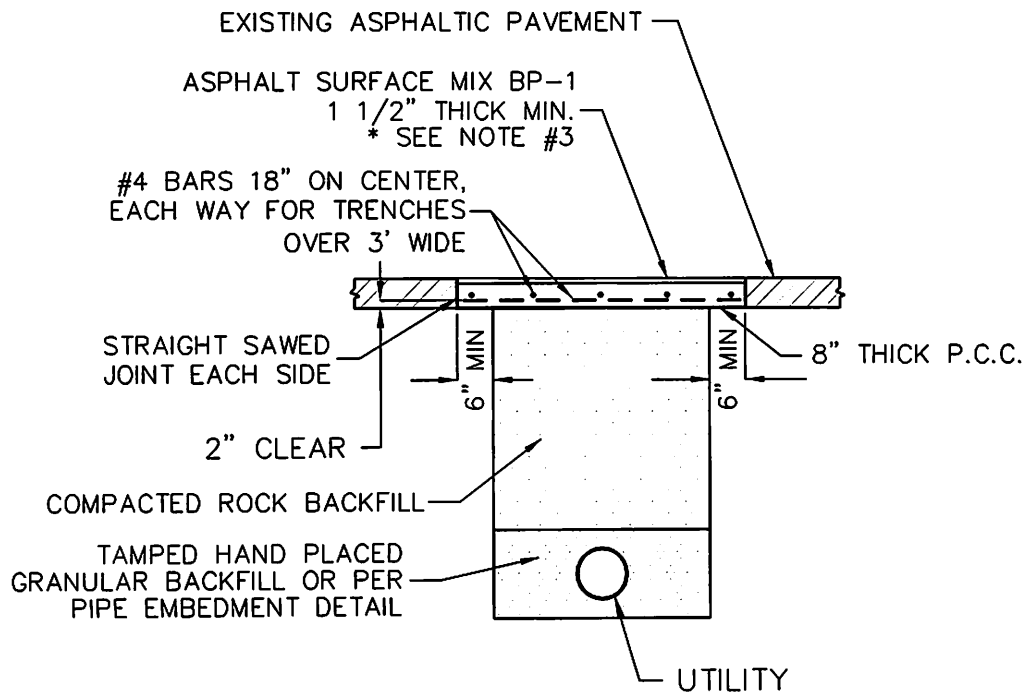
1. EXPANSION JOINTS SHALL BE INSTALLED AT CURB RETURN RADIUS POINTS AND AT DRIVE APPROACHES, THESE 1/2" FELT EXPANSION JOINTS SHALL HAVE 5/8" DIA. x 18" LONG SMOOTH DOWELS. THESE DOWELS SHALL BE GREASED AND WRAPPED ON ONE END WITH EXPANSION TUBES.
2. CONTROL JOINTS/CONTROL JOINTS MAY BE SAWED, OR PRE-FORMED.
- * 3. CURB PAN CAN BE CONSTRUCTED AT 2.0% CROSS SLOPE ON COMMERCIAL ROADWAY WHEN ASPHALT OPTION IS USED UNDER PROPOSED CURB, AND WHEN THE ROADWAY AND CURB IS CONSTRUCTED MONOLITHICALLY.
- * 4. WHEN PLANS ARE NOTED TO CONSTRUCT A TILT OUT PAN. THE GUTTER PAN SHALL BE TILTED OUT NO MORE THAN THE ADJOINING STREET CROSS SLOPE.

TYPE "A" CURB AND GUTTER

REVISION AND DESCRIPTION		NO:	DATE:
ADDED NOTE #4		1	8/2007
REMOVED ALTERNATE RADIUS DIMENSIONS		2	3/2009
TYPE "A" CURB & GUTTER		STANDARD DETAILS	
CHECKED BY:	DATE:	DRAWN BY:	DATE:
SHEET NUMBER			
7 OF 8			
SECTION			
20.07			



TYPICAL CONCRETE STREET SECTION



TYPICAL ASPHALT ROADWAY

NOTES:

1. ALL CONCRETE USED FOR PATCHING SHALL BE HIGH EARLY STRENGTH, AND MAY CONTAIN UP TO 4.0% NON-CALCIUM ACCELERATOR. (NON-CALCIUM ACCELERATOR SHALL BE APPROVED BY THE ENGINEER)
2. ALL CONCRETE MIXES MUST CONTAIN AIR ENTRAINMENT.
3. DURING WINTER MONTHS WHEN HOT MIX ASPHALT IS NOT AVAILABLE ONE OF TWO PROCESSES SHALL OCCUR FOR STREET CUTS:
 - A. IF EXISTING STREET OVERLAY IS MORE THAN 4 YEARS OLD: THE CONTRACTOR SHALL FILL THE STREET CUT TO THE SURFACE WITH CONCRETE AND NO ASPHALT CAP WILL BE PLACED. IN THIS CASE THE CONCRETE SHALL BE 9 1/2" THICK.
 - B. IF THE STREET HAS AN OVERLAY WHICH IS LESS THAN 4 YEARS OLD: THE CONTRACTOR SHALL CAP THE CUT WITH COLD MIX ASPHALT AND WILL THEN RETURN IN THE SPRING, WHEN HOT MIX IS AVAILABLE, AND REPLACE THE COLD MIX WITH HOT MIX.

NO:	DATE:	REVISION AND DESCRIPTION
1	3/2009	ADDED NOTE #3

STREET CUT REPAIR FOR PATCHING & BACKFILLING PAVED STREETS & ALLEYS	STANDARD DETAILS
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CHECKED BY:	DRAWN BY:	DATE:
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Jefferson
 DEPARTMENT OF PUBLIC WORKS
 ENGINEERING DIVISION

SHEET NUMBER
8 OF 8
 SECTION
20.08

SIDEWALK NOTES:

The minimum width of continuous passage shall be 48 inches.

The cross slope of a sidewalk shall not exceed 2% ($\frac{1}{4}$ " per foot). The slope may be toward or away from the street as needed to achieve positive drainage.

The sidewalk shall have sawed joints of a depth that is equal to $\frac{1}{3}$ of the the sidewalk thickness. The joints are to be sawed at a spacing that is equal to the width of the sidewalk, or as directed by the engineer. In no instance shall the spacing measured in feet, exceed twice the sidewalk thickness measured in inches.

In all instances where the sidewalk crosses through a driveway or drive approach it shall be a minimum of 6" thick.

All sidewalks are to be constructed on a compacted subgrade (4" min). This requirement may be met with native material or granular material supplied by the contractor.

All concrete used for sidewalk construction shall have a 28 day compressive strength of 4000 psi.


The sidewalk surface shall have a non-slip broom finish that is perpendicular to the sidewalk.

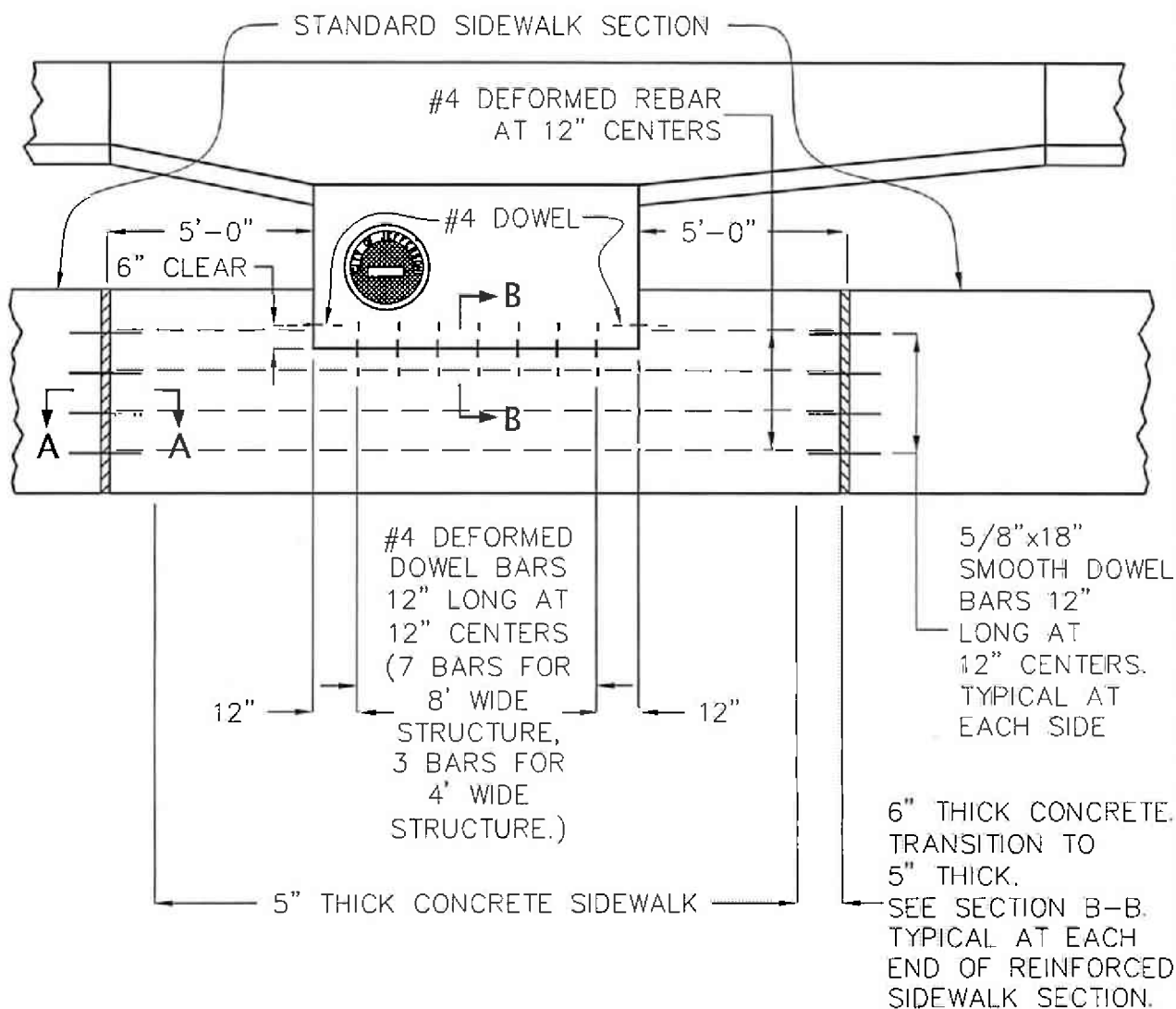
Sidewalks shall be cured as specified in section 502.6 of the 2004 Missouri Standard Specifications for Highway Construction.

Expansion joints shall be $\frac{1}{2}$ " pre molded joint filler (non extruding, Type B) and shall be placed as follows:

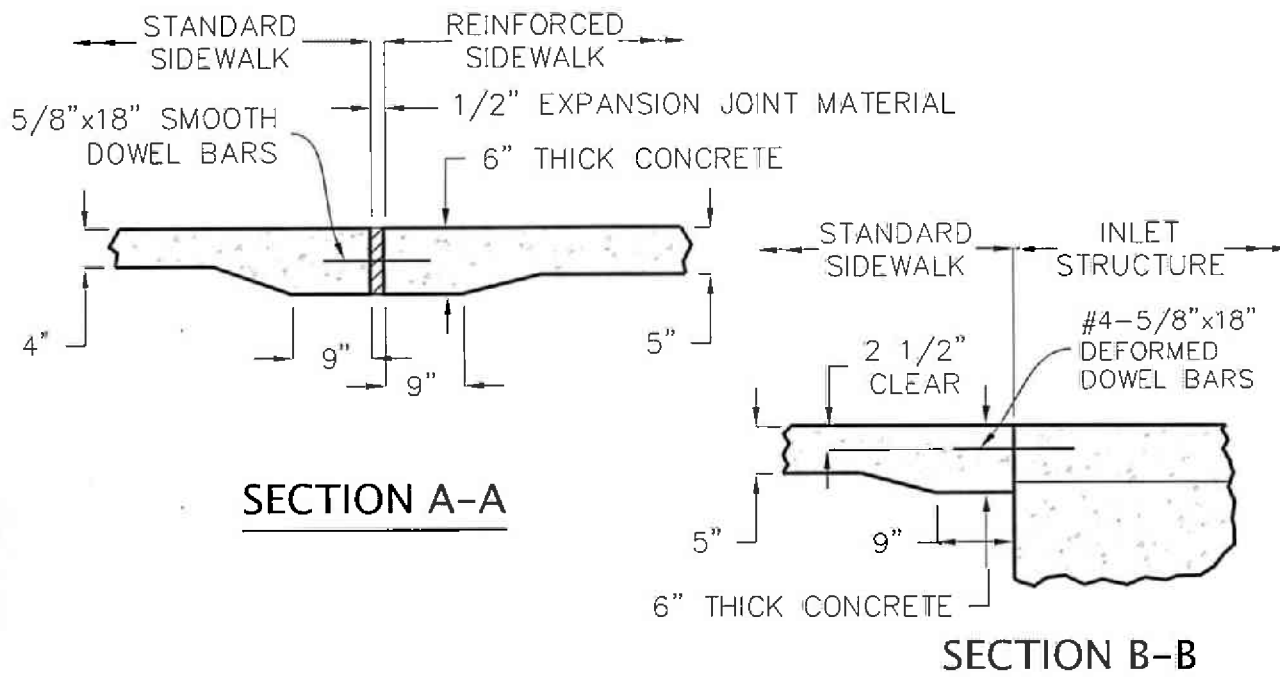
- a. in long runs at a maximum spacing of 300 feet
- b. where one sidewalk abuts another or the sidewalk abuts a sidewalk ramp
- c. where the sidewalk is parallel and adjacent to a rigid structure, not including curb and gutter.


For A.D.A. ramp information, see Section: 23 in the City of Jefferson Standard Drawings Book for A.D.A. ramp specifications and details.

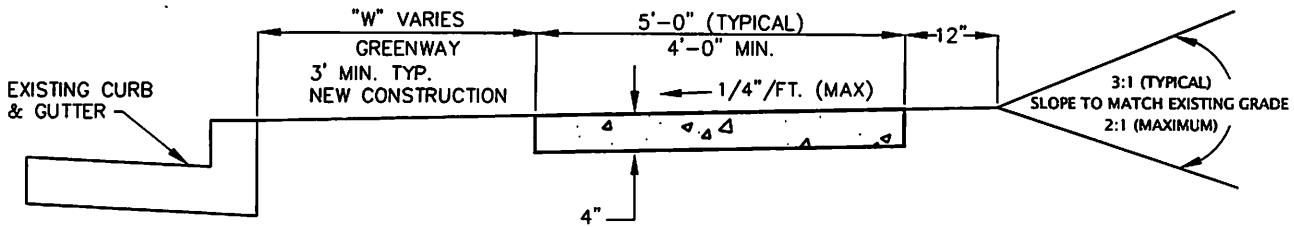
NO:		DATE:	REVISION AND DESCRIPTION	
SIDEWALK NOTES			STANDARD DETAILS	
CHECKED BY:	DRAWN BY:		DATE:	
				
SHEET NUMBER 1 OF 5 SECTION 22.01				



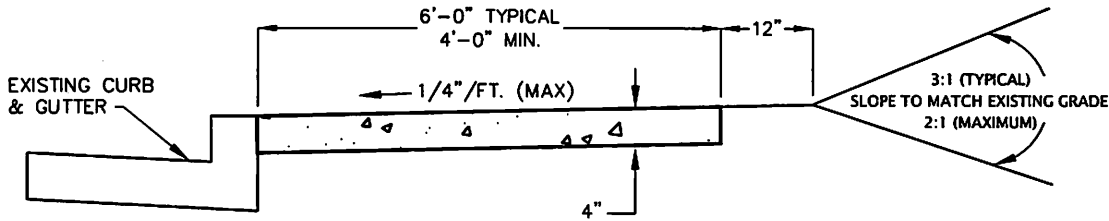
PLAN VIEW AND REINFORCEMENT FOR SIDEWALKS AT STORMWATER INLET/JUNCTION BOX LOCATIONS



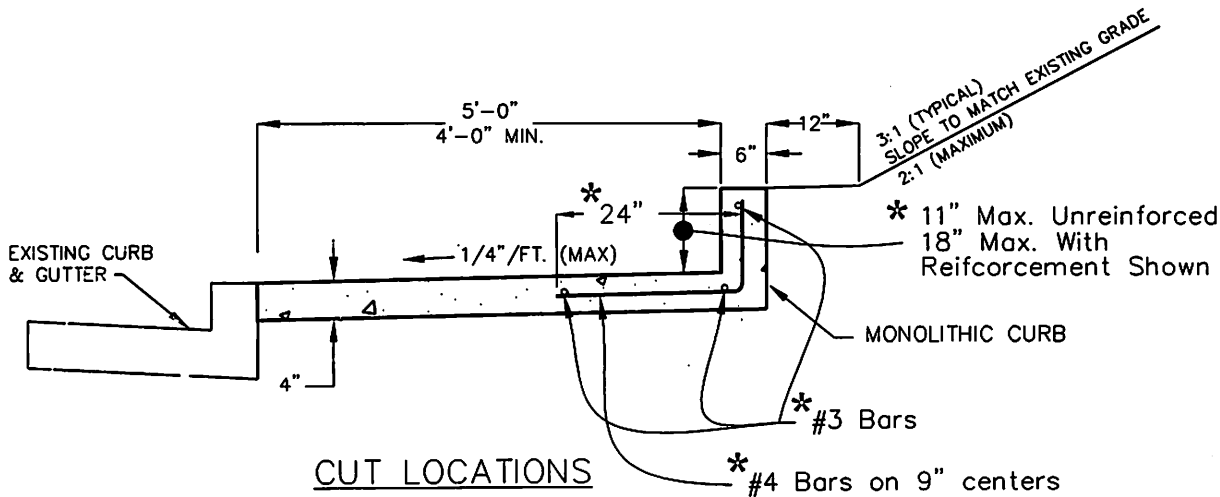
REVISION AND DESCRIPTION		
NO:	DATE:	NEW REQUIREMENT FOR SIDEWALKS AT INLET LOCATIONS
1	1/2010	
REINFORCEMENT DETAILS FOR SIDEWALK AT INLET LOCATIONS		
CHECKED BY:	DRAWN BY:	DATE:
<div>  Jefferson DEPARTMENT OF PUBLIC WORKS ENGINEERING DIVISION </div>		
SHEET NUMBER 2 OF 5 SECTION 22.02		



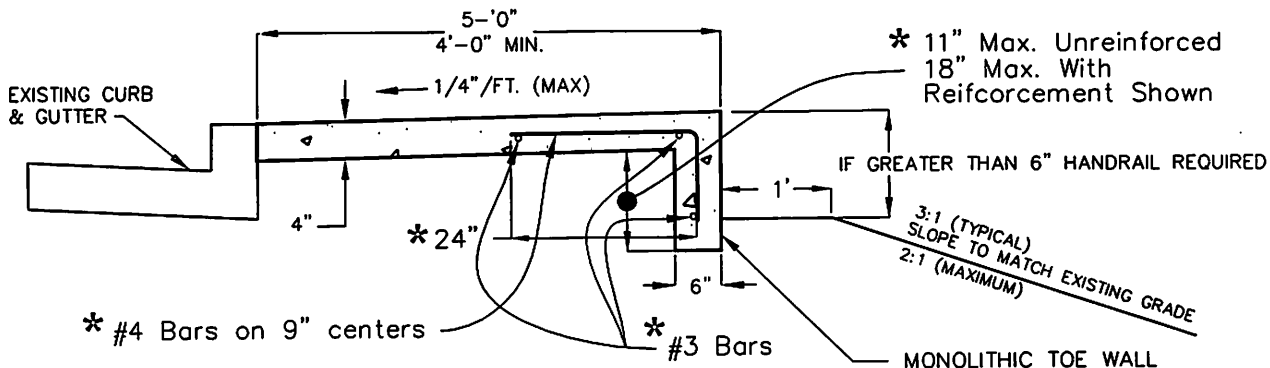
GREENWAY SECTION



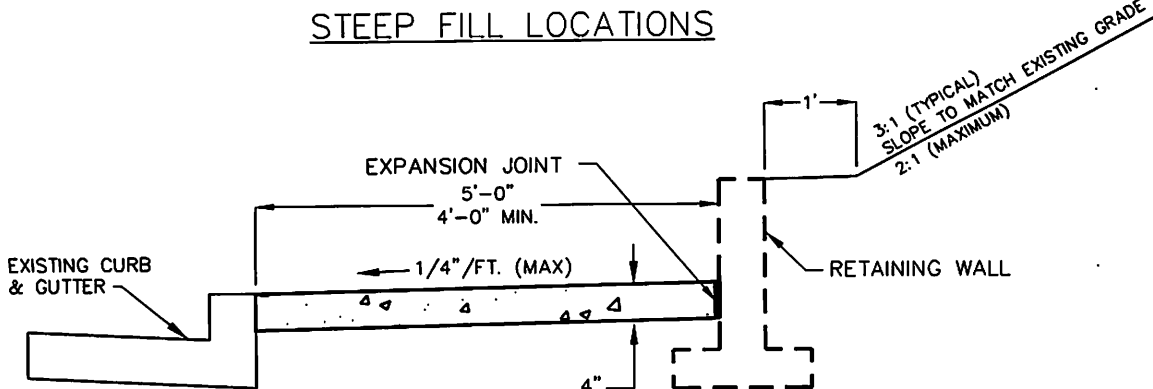
SIDEWALK AT BACK OF CURB SECTION



CUT LOCATIONS



STEEP FILL LOCATIONS



SIDEWALK WITH RETAINING WALL

NO:	DATE:	REVISION AND DESCRIPTION
1	1/2013	* ADDED REINFORCEMENT TO CURB WALLS

TYPICAL SIDEWALK SECTIONS	STANDARD DETAILS
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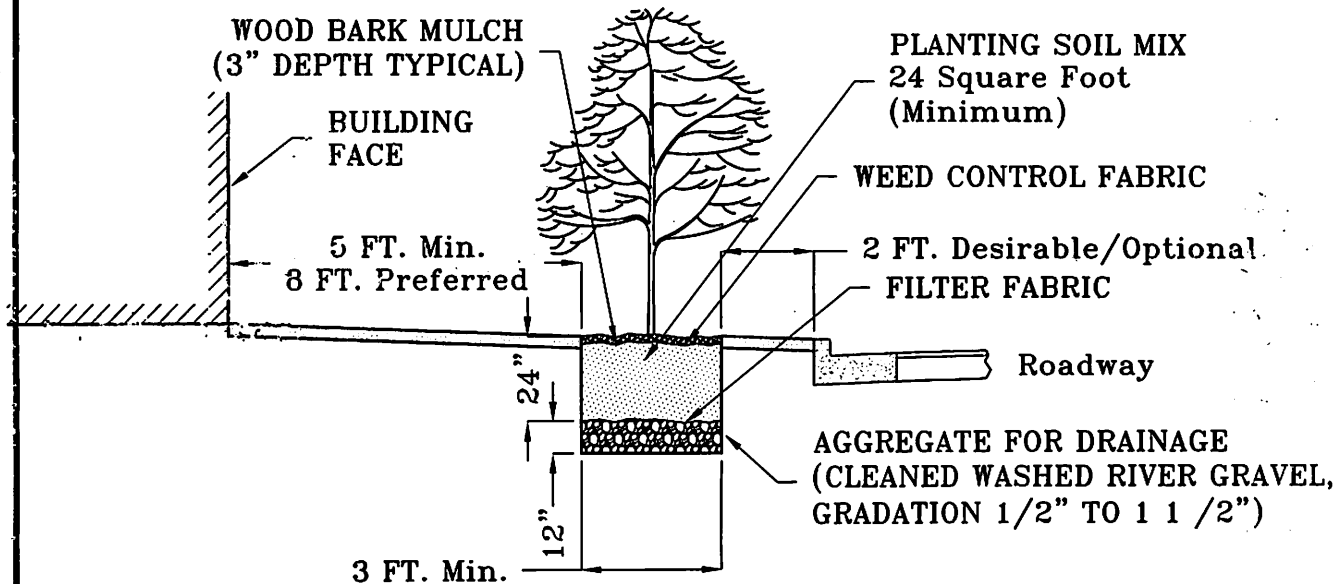
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DOWNTOWN SIDEWALK FINISH

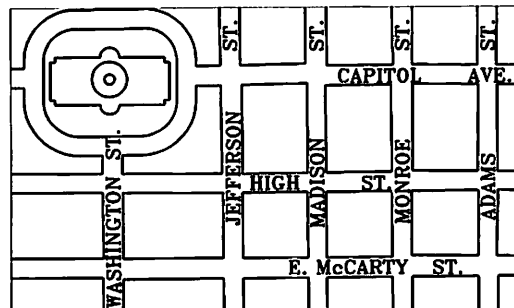
The sidewalk finish shall be an exposed Osage River aggregate. This finish is to be achieved on all exposed (non-colored) concrete construction including sidewalks, exposed foundations and accessible ramps.

Exposed Aggregate Finish: Expose Osage River aggregates and sand of pavement surfaces as follows:

1. Immediately after floating, spray-apply chemical surface retarder to pavement according to the manufacturer's written instructions.
 2. Cover with plastic sheeting, sealing laps with tape, and remove when ready to continue finishing operations.
 3. Without dislodging aggregate, remove excess mortar by lightly brushing surface with a stiff, nylon bristle broom.
 4. Fine-spray surface with water and brush. Repeat water flushing and brushing cycle until cement film is removed from aggregate surfaces to depth required.
 5. Exposed Aggregate Concrete shall be sealed. Sealer to be approved by the Engineer.
- Contractor may achieve the exposed Osage River aggregate finish by other methods upon approval by the engineer.
 - Curb/Gutter and driveway approaches shall be of plain concrete, or other color variation approved by the director
 - All concrete shall have a 28 day compressive strength of 4000 psi.



DOWNTOWN TREE WELL AND PROMENADE DETAIL



Area Where Exposed
Aggregate Finish is Required

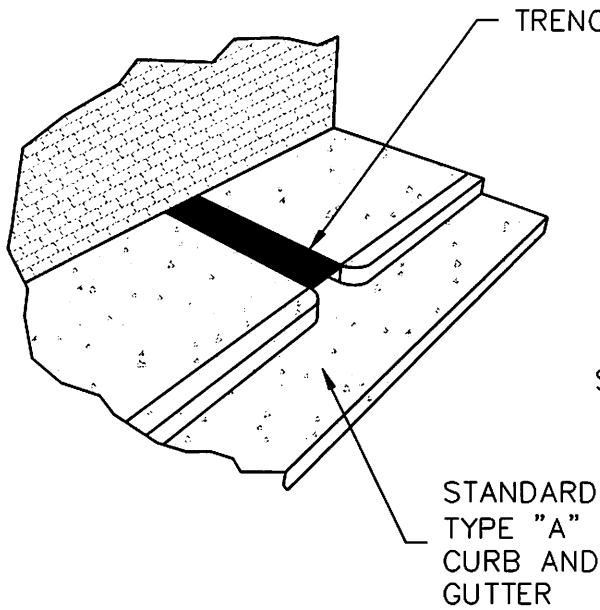
REVISION AND DESCRIPTION

CITY OF JEFFERSON
DOWNTOWN TREE WELL &
PROMENADE DETAILS

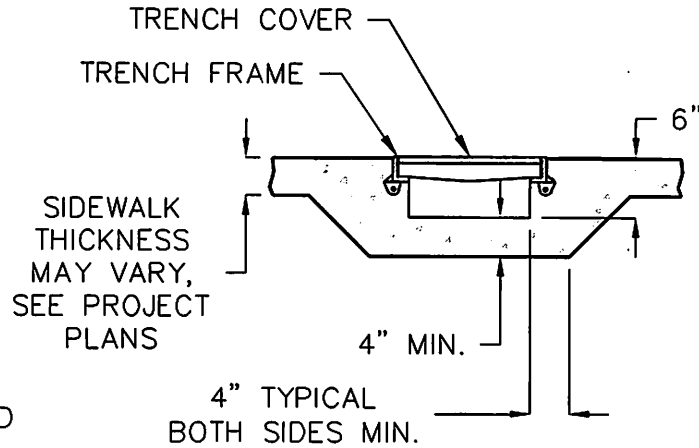
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Jefferson
CITY OF JEFFERSON
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION

SHEET NUMBER
4 OF 5
SECTION
22.04



ISOMETRIC VIEW



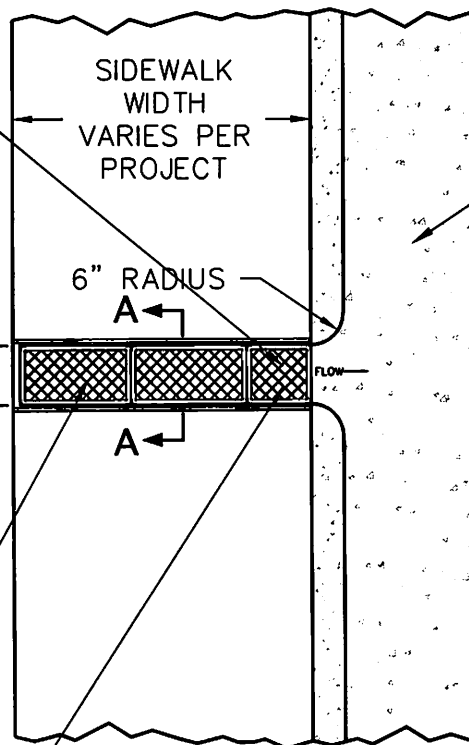
SECTION A-A

IF THE SIDEWALK IS NOT OF AN EVEN WIDTH, A SPECIAL CUT TRENCH COVER SHALL BE ORDERED FROM THE MANUFACTURE.

TRENCH COVER = 14"x24"
TRENCH AREA = 12" WIDE

SIDEWALK TRENCH COVER SHALL BE DEETER CATALOG NO. 3990-D, STYLE D: SOLID, WITH DEETER FRAME, (OR EQUAL MATERIAL APPROVED BY THE ENGINEER).

MANUFACTURED TRENCH COVERS ARE IN 2 FT. SECTIONS.



**SIDEWALK TRENCH DRAIN
(Plan View)**

REVISION AND DESCRIPTION

DATE:

NO:

SIDEWALK TRENCH DRAIN

CHECKED BY:

DRAWN BY:

DATE:

City of Jefferson
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION

SHEET NUMBER
5 OF 5
SECTION
22.05

A.D.A. RAMP NOTES:

Sidewalk ramps shall be provided at all street intersections and commercial drive approaches where there is existing or proposed sidewalk and curb and as directed by the engineer. All ramps inclusive of the flares shall be 6" thick.

Sidewalk ramps shall be a minimum of 48" wide exclusive of the flared sides. The ramps are to have a maximum longitudinal slope of 1:12. In the instance where an intersecting sidewalk abuts a sidewalk ramp the slope of the ramp flare shall be a maximum of 1:10.

All ramps shall have a landing a minimum of 48" square whose slope does not exceed 2% in any direction.

Sidewalk ramps should not be aligned with drainage structures.

All ramps shall be constructed prior to the sidewalk construction unless otherwise approved by the engineer.

A.D.A. RAMP DETECTABLE WARNING PANELS

All A.D.A. ramps, in locations where the sidewalk intersects a commercial entrance having a traffic control sign or signal, or where the sidewalk intersects a street, or alley, shall have detectable warning panels.

Place truncated A.D.A. ramp detectable warning panels at the base of the curb ramp. Install panels across full width of ramp, a minimum of 24" in depth, and set back 2"-6" from the back of curb/bottom of ramp. (see ramp details)

At ramps abutting large curb radii where the transition area between the back of curb and the start of the ramp exceeds 5 feet the detectable warning panels shall be placed radially to the curb.

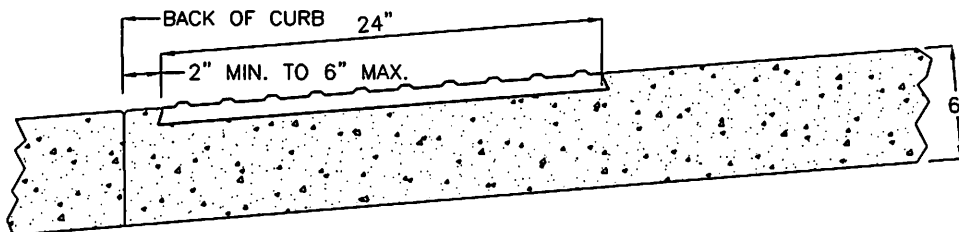
The proposed panels shall be one of the following:

1. MASCO CASTinTACT warning panels as manufactured by Masons Supply Company, (503-234-4321).
2. Detectable Warning Plates as manufactured by Neenah Foundry Company, (800-558-5075).
3. ADA Arcis Tactile panels as manufactured by Arcis Corp., (503-647-5042).

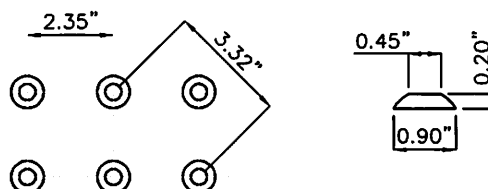
Other panels may be used upon approval by the city engineer.

The Contractor shall follow the manufactures recommendations for the installation of the panels. The panels shall match the slope of the ramp and shall be placed such that no vertical displacement occurs between the panel and the surrounding concrete.

The panels are to be red in color, and may be made up of 2'x2', 2'x2.5', or 2'x3' panels.



CROSS SECTION OF DETECTABLE WARNING PANEL



AMERICANS WITH DISABILITIES ACT 4.29.2 STANDARDS

TRUNCATED DOME DETAILS

REVISION AND DESCRIPTION

ADDED OPTIONAL A.D.A. PANEL NOTES

DATE:

3/2009

NO:

**1

A.D.A. RAMP
INFORMATION/DETAILS

STANDARD DETAILS

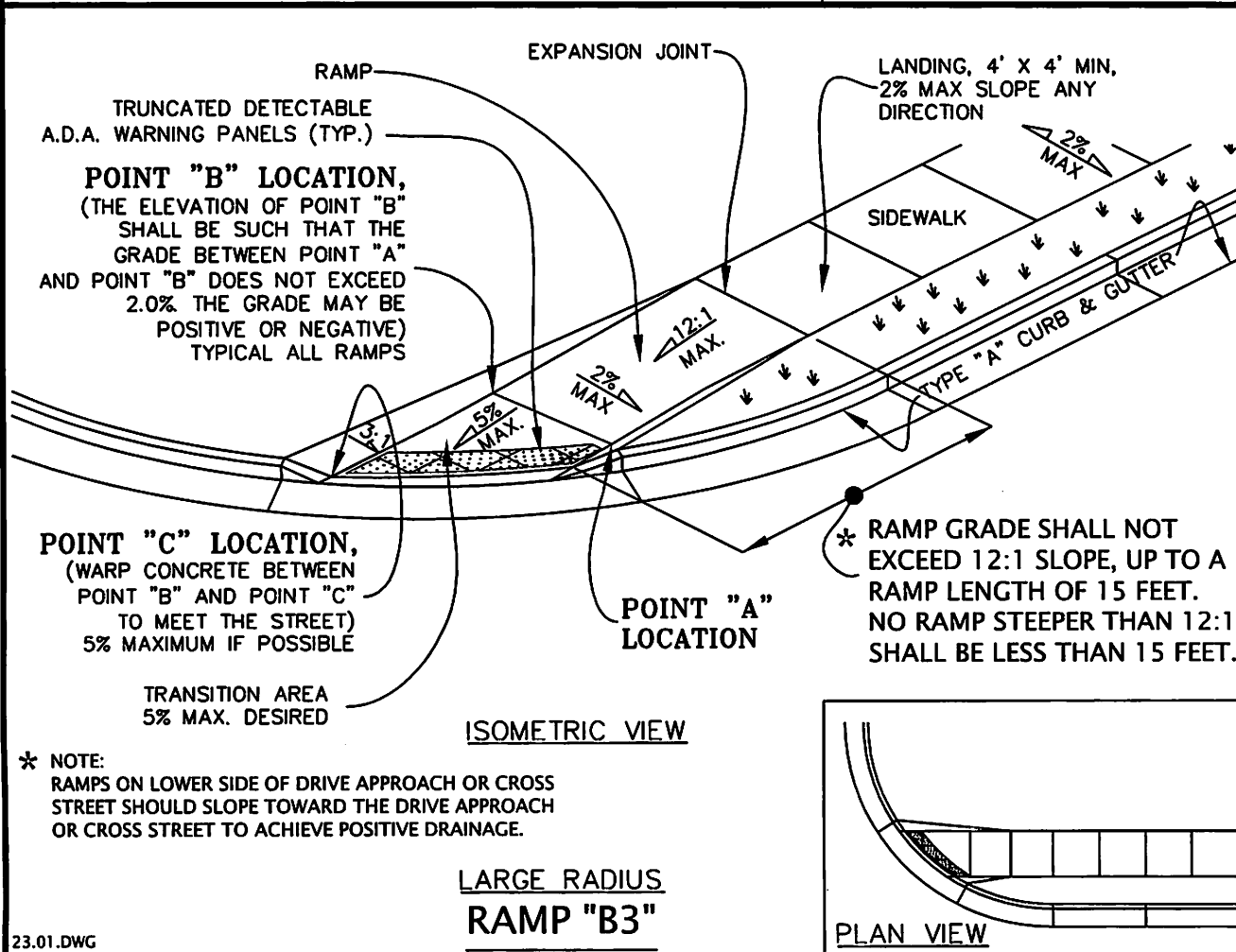
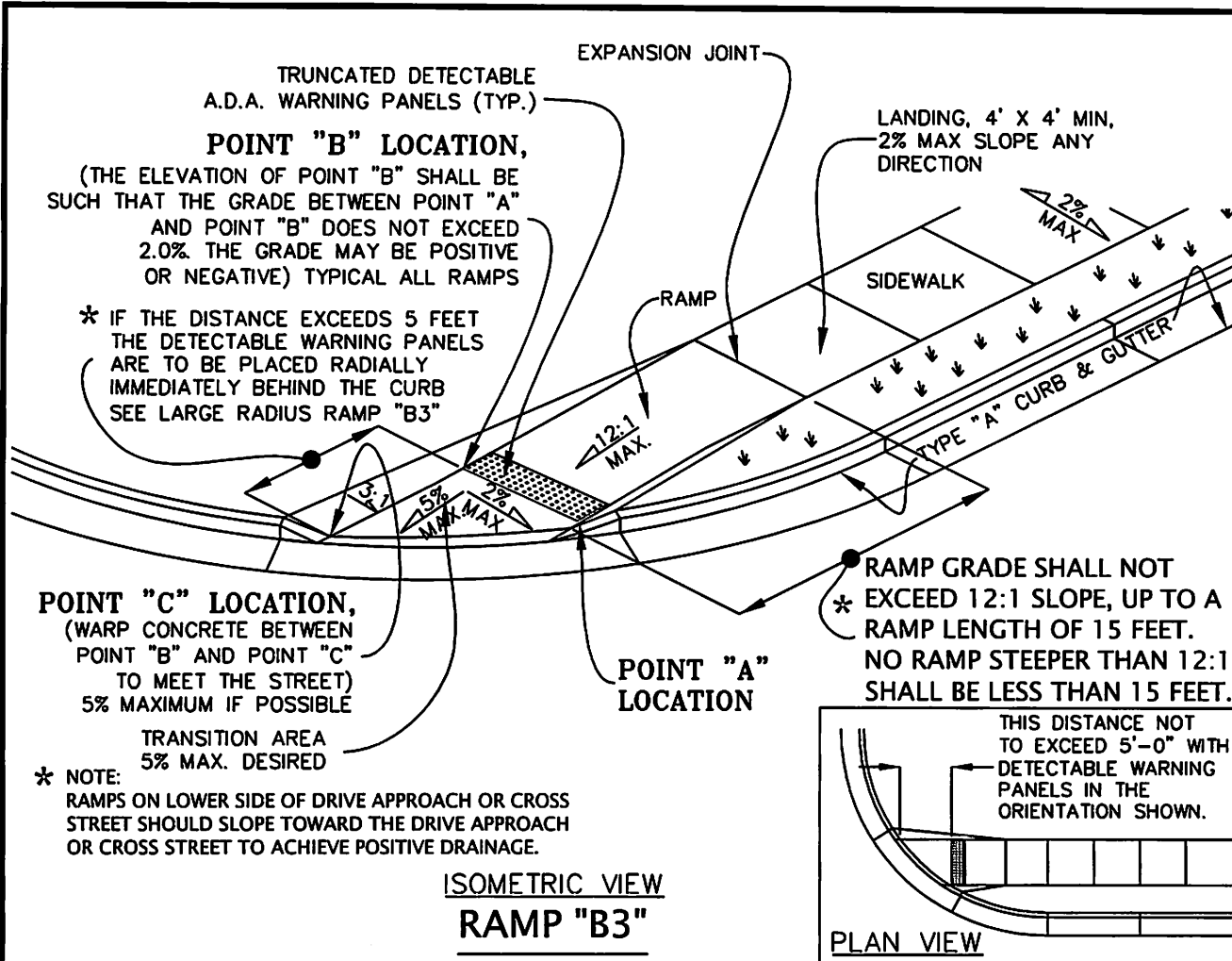
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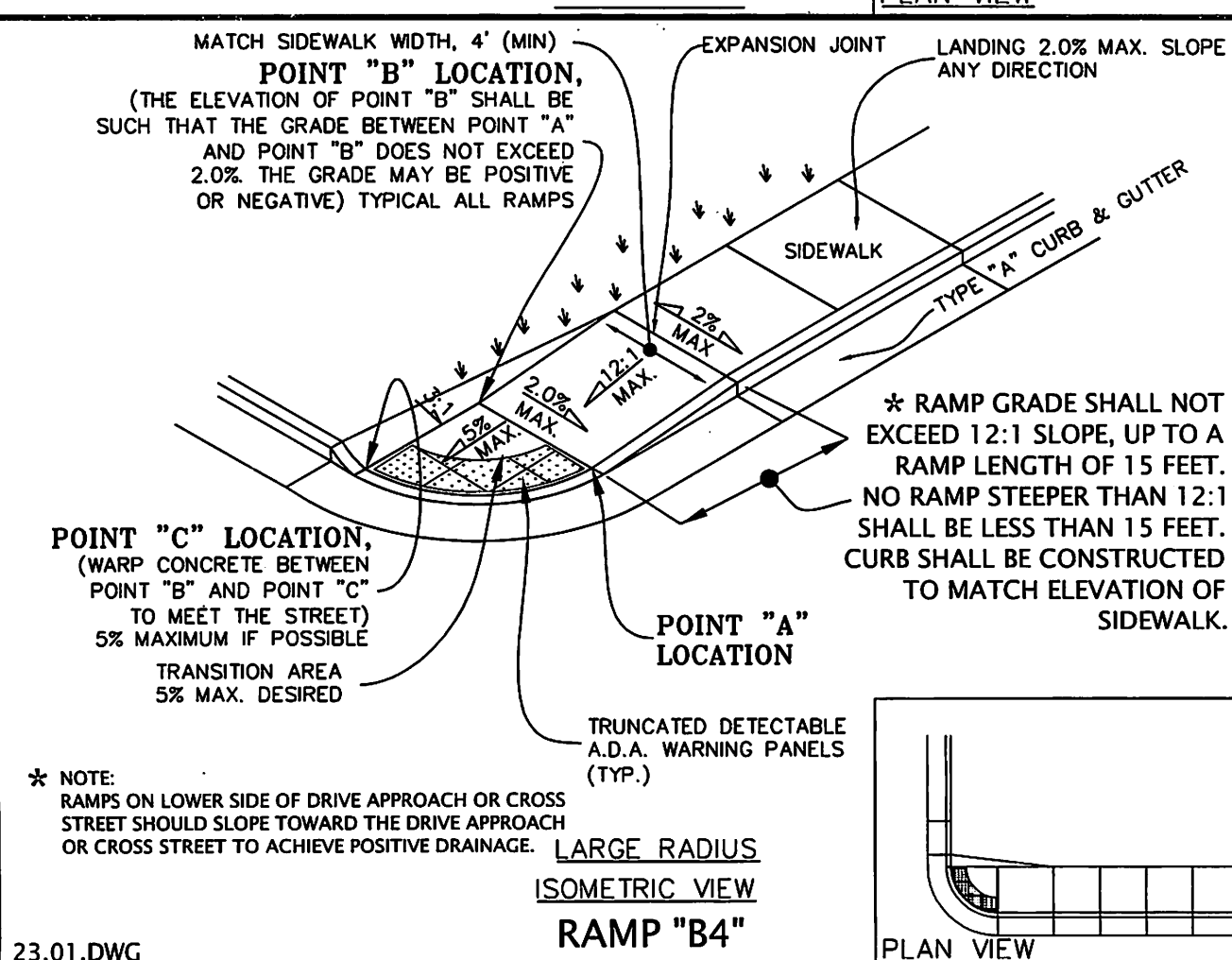
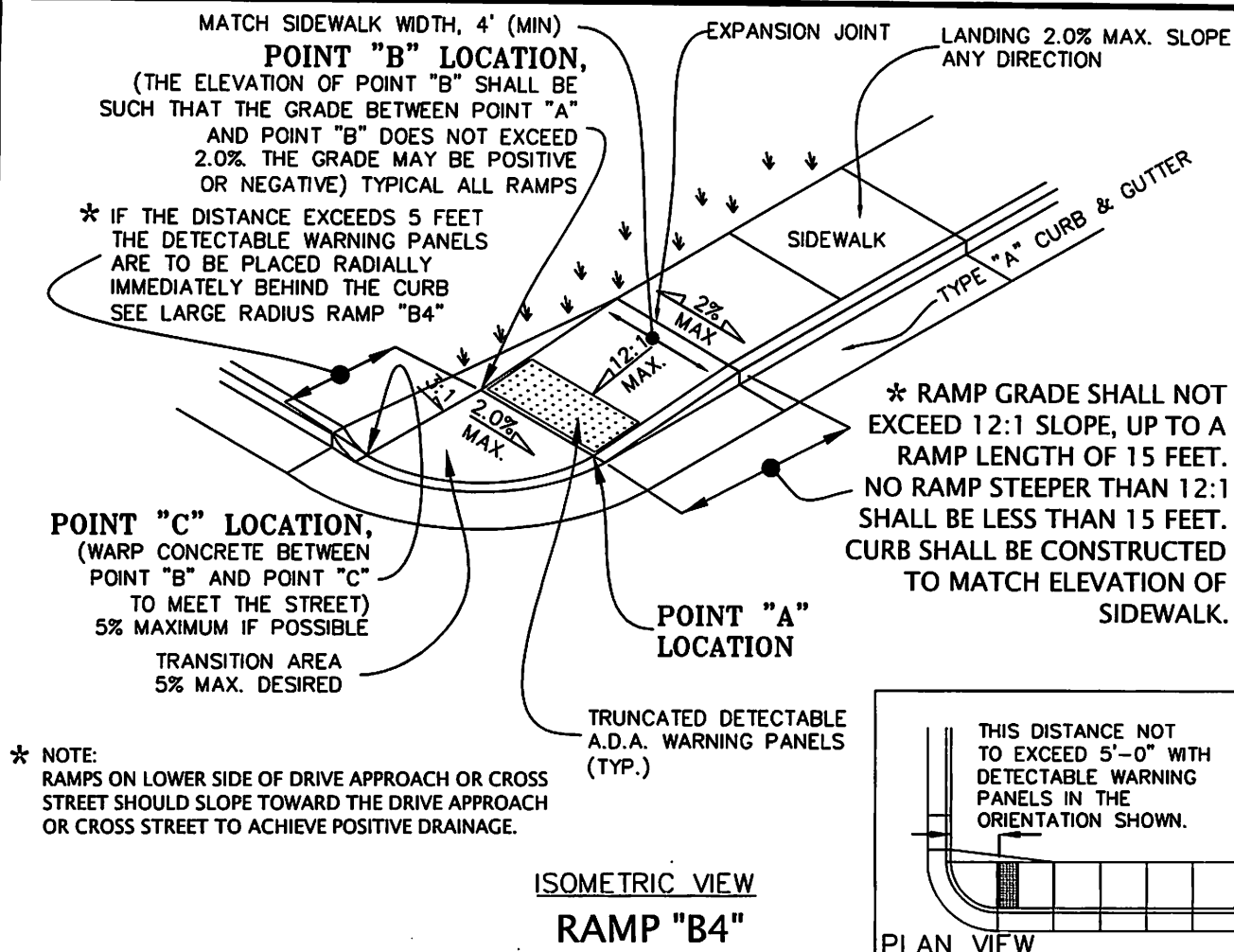
DATE:



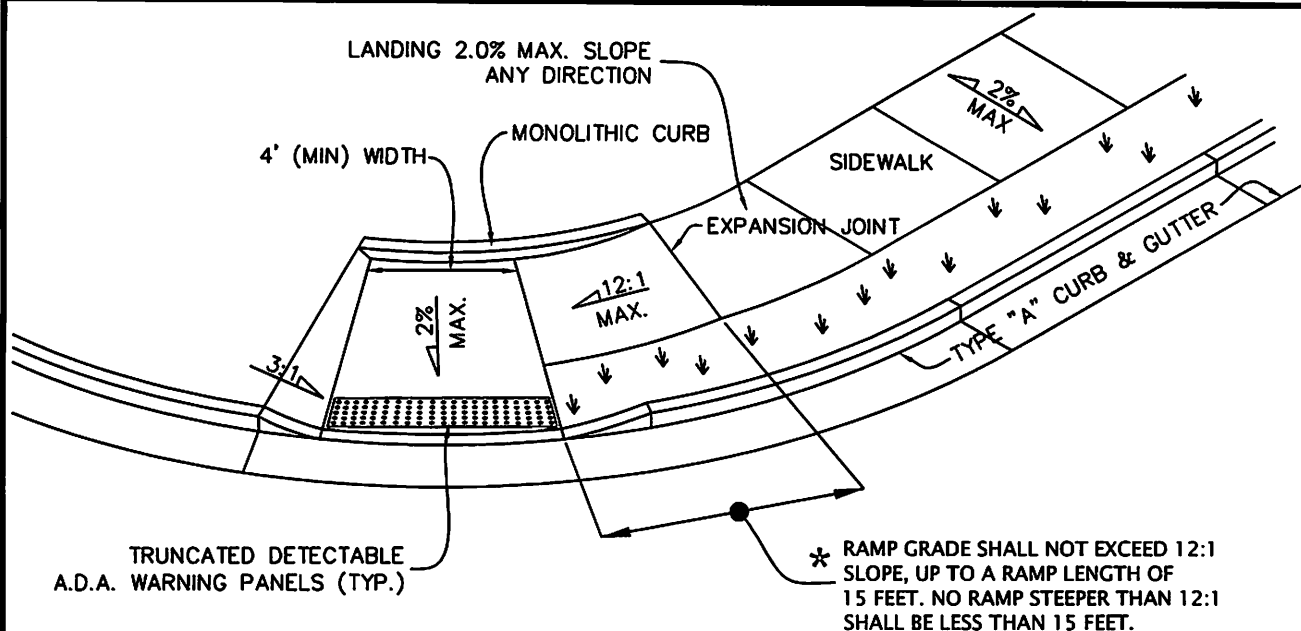
SHEET NUMBER
1 OF 8
SECTION
23.01



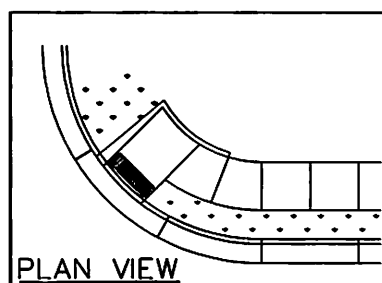
SHEET NUMBER 5 OF 8 SECTION 23.05	 Jefferson DEPARTMENT OF PUBLIC WORKS ENGINEERING DIVISION	CHECKED BY:			SIDEWALK RAMP DETAILS	NO:	DATE:	REVISION AND DESCRIPTION
		DRAWN BY: DPB						
		DATE: 3-06			STANDARD DETAILS			



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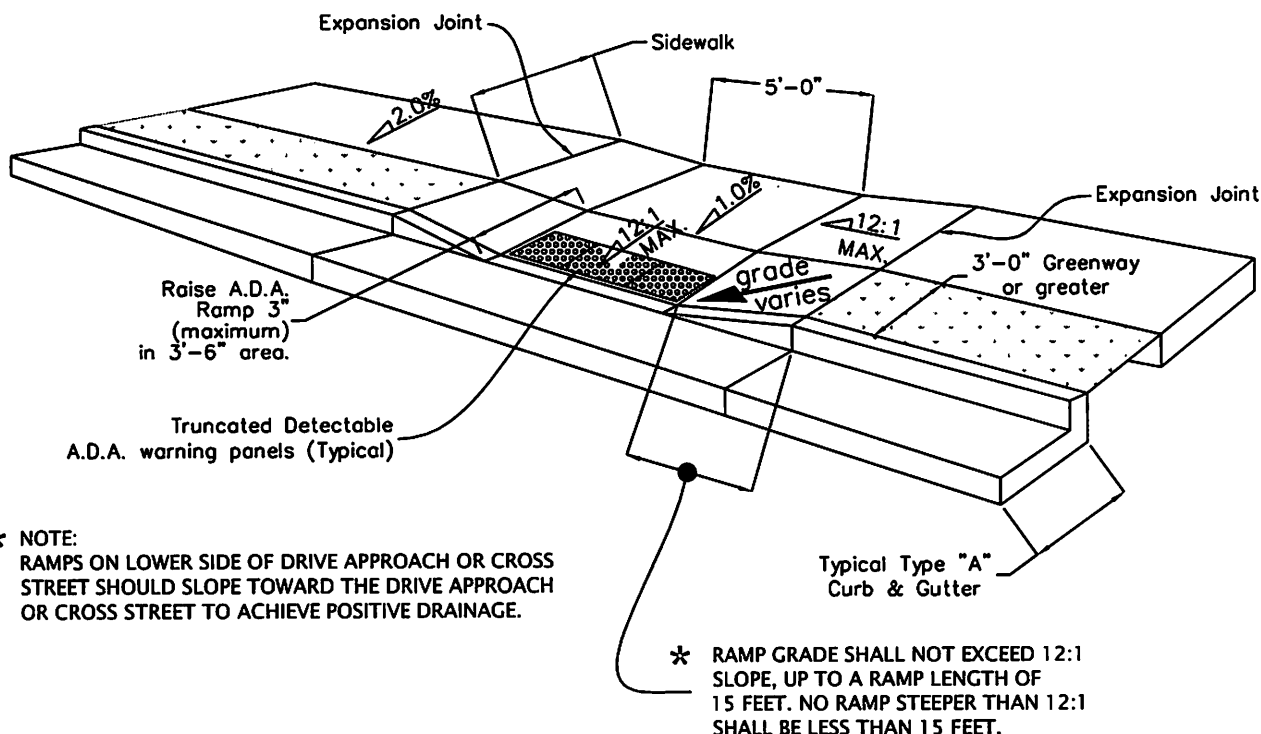
ISOMETRIC VIEW
RAMP "C4"



NO.	DATE:	REVISION AND DESCRIPTION
1	1/2013	* NOTE: A.D.A. RAMP GRADES/LENGTHS

SIDEWALK RAMP DETAILS	STANDARD DETAILS
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CHECKED BY:	DRAWN BY: DPB	DATE: 3-06
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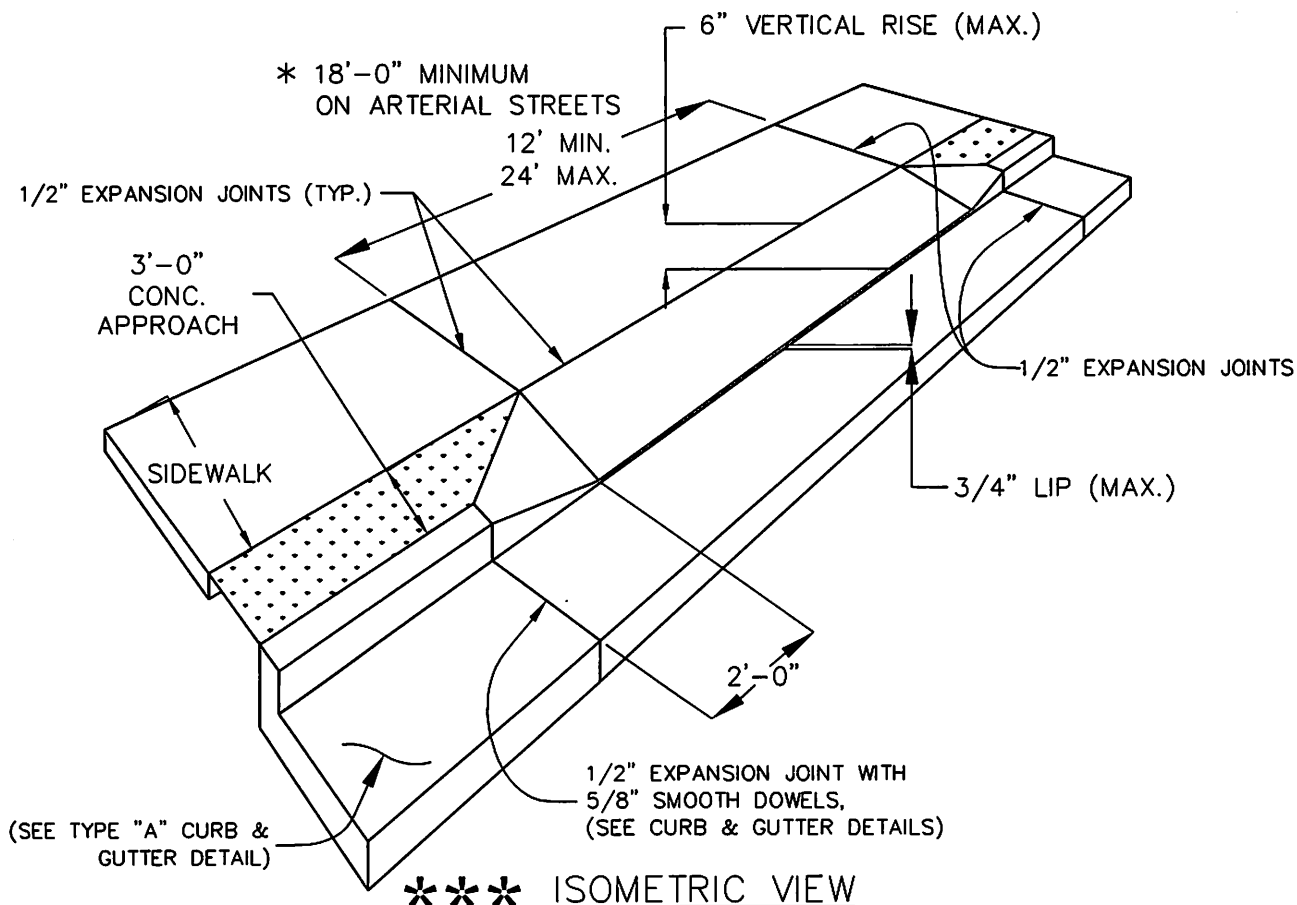
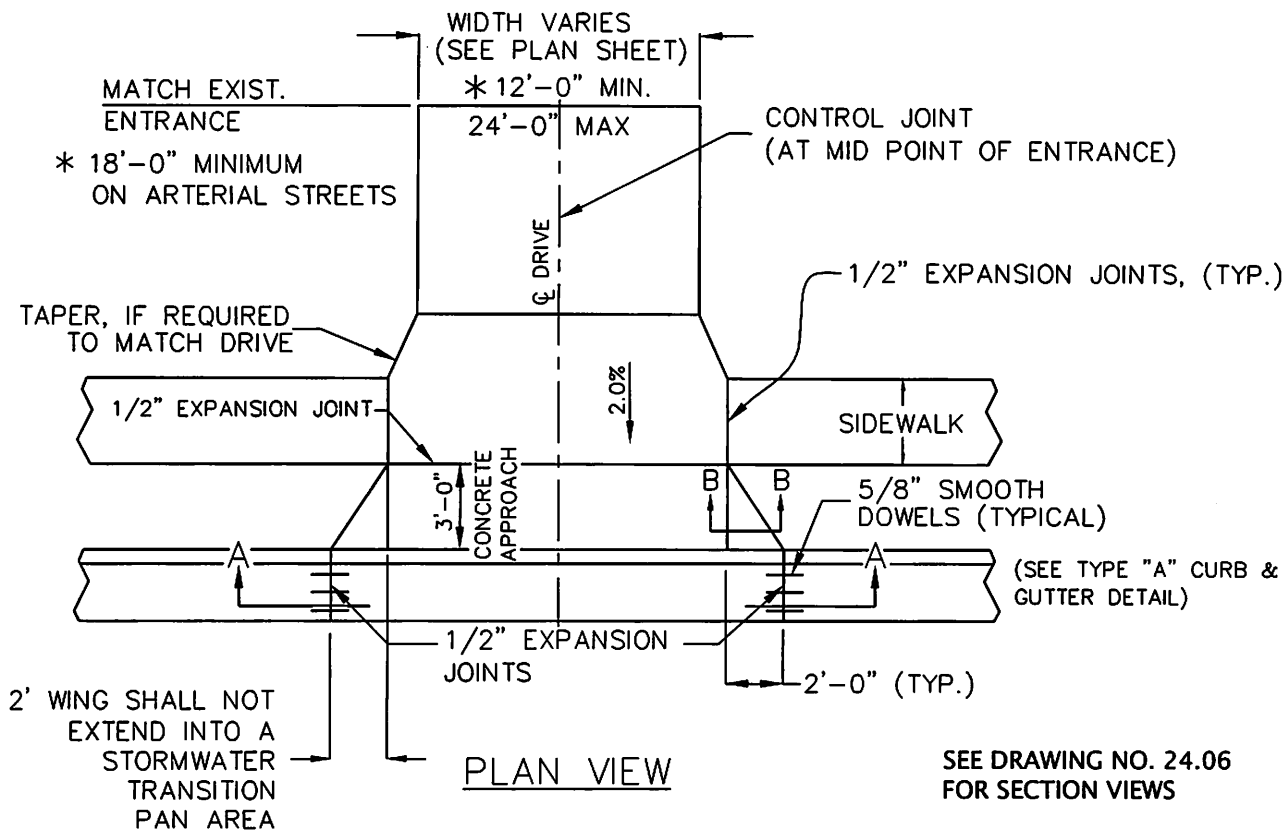
* NOTE:
RAMPS ON LOWER SIDE OF DRIVE APPROACH OR CROSS STREET SHOULD SLOPE TOWARD THE DRIVE APPROACH OR CROSS STREET TO ACHIEVE POSITIVE DRAINAGE.

NOTES:
See Ramp "C3" if Greenway width is less than 3'-0"

ISOMETRIC VIEW
RAMP "C5"



SHEET NUMBER
8 OF 8
SECTION
23.08



RESIDENTIAL DRIVE APPROACH LAYOUT FOR NEW ROADWAY CONSTRUCTION

24.01.DWG

NO.	DATE:	REVISION AND DESCRIPTION
1	1/2010	*** ADDED ISOMETRIC VIEW

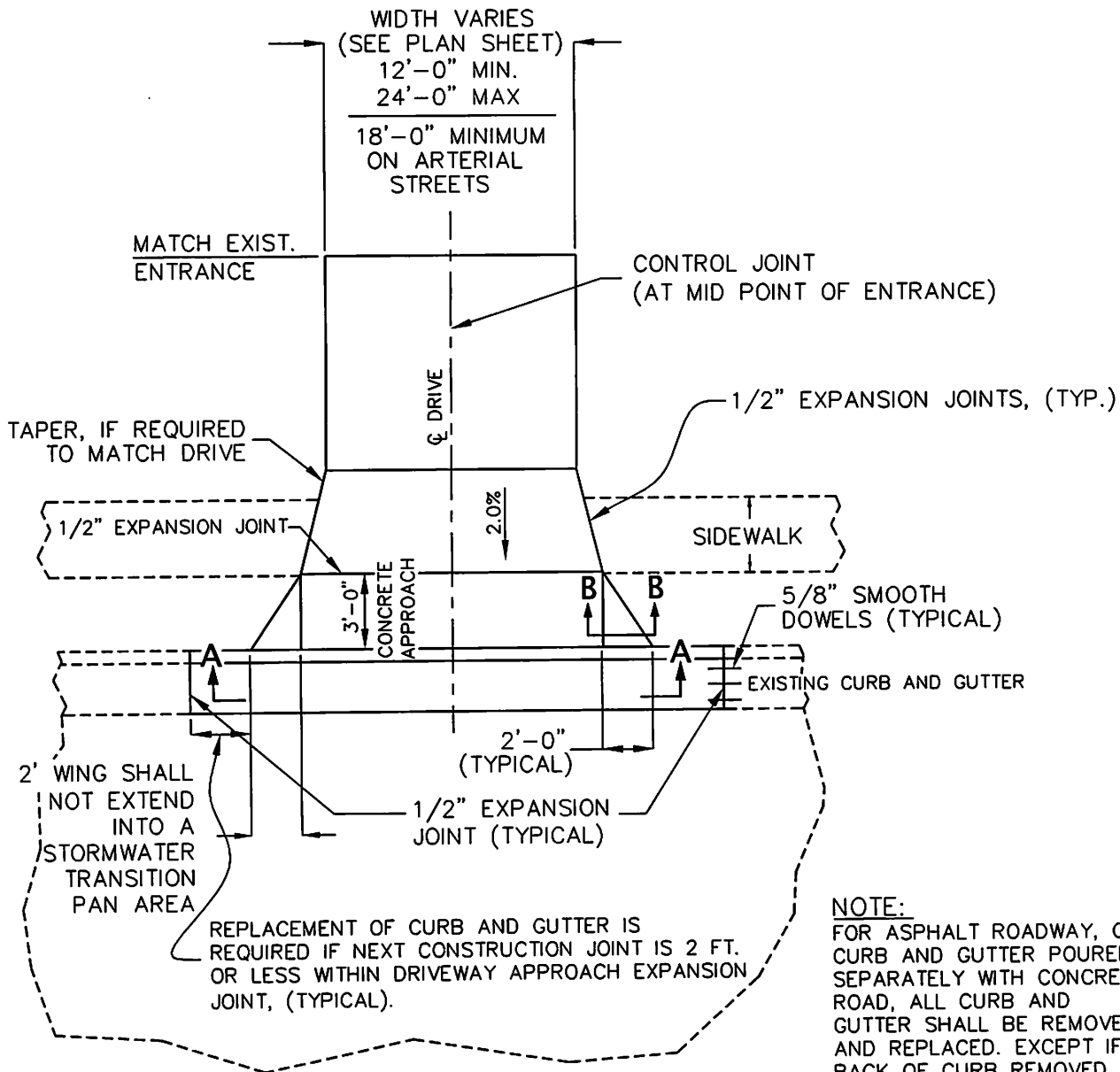
RESIDENTIAL DRIVE
APPROACH DETAILS
FOR NEW ROADWAY
CONSTRUCTION

CHECKED BY:	DRAWN BY:	DATE:
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<p>City of Jefferson DEPARTMENT OF PUBLIC WORKS ENGINEERING DIVISION</p>
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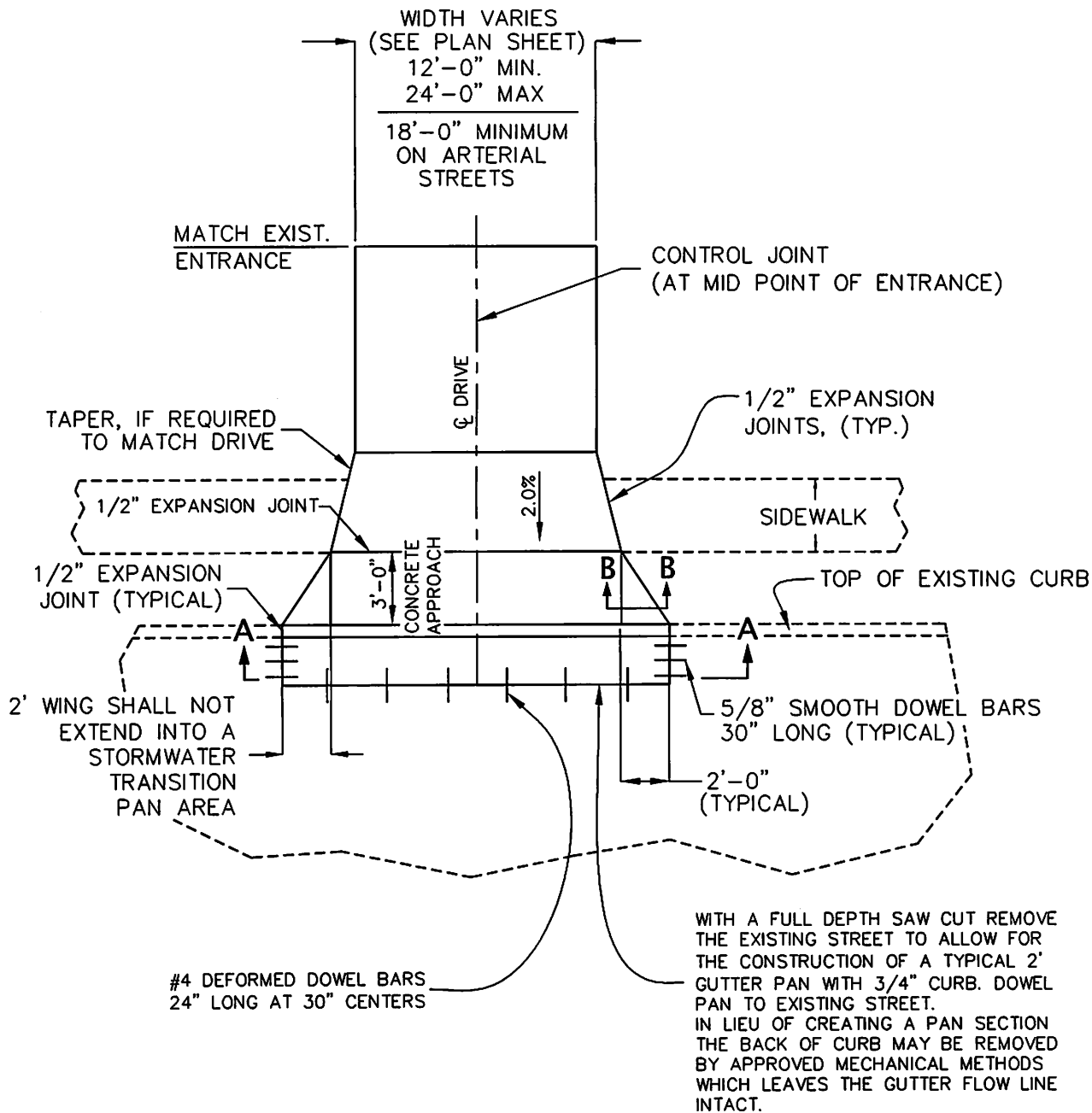
SHEET NUMBER
1 OF 7
SECTION
24.01



NOTE:
FOR ASPHALT ROADWAY, OR CURB AND GUTTER POURED SEPARATELY WITH CONCRETE ROAD, ALL CURB AND GUTTER SHALL BE REMOVED AND REPLACED. EXCEPT IF BACK OF CURB REMOVED BY APPROVED MECHANICAL METHODS.

RESIDENTIAL DRIVE APPROACH
LAYOUT FOR EXISTING CONCRETE OR
ASPHALT ROADWAY WITH
CONCRETE CURB & GUTTER

REVISION AND DESCRIPTION		
NO:	DATE:	
RESIDENTIAL DRIVE APPROACH DETAILS FOR EXISTING CONCRETE OR ASPHALT ROADWAYS WITH CURB AND GUTTER		
CHECKED BY:	DRAWN BY:	DATE:
<div style="text-align: center;">  Jefferson <small>CITY OF</small> <small>DEPARTMENT OF PUBLIC WORKS</small> <small>ENGINEERING DIVISION</small> </div>		
SHEET NUMBER		
2 OF 7		
SECTION		
24.02		



RESIDENTIAL DRIVE APPROACH
LAYOUT FOR EXISTING MONOLITHIC
CONCRETE ROADWAY

NO:	DATE:	REVISION AND DESCRIPTION

RESIDENTIAL DRIVE
APPROACH DETAILS
FOR EXISTING
MONOLITHIC
CONCRETE ROADWAYS

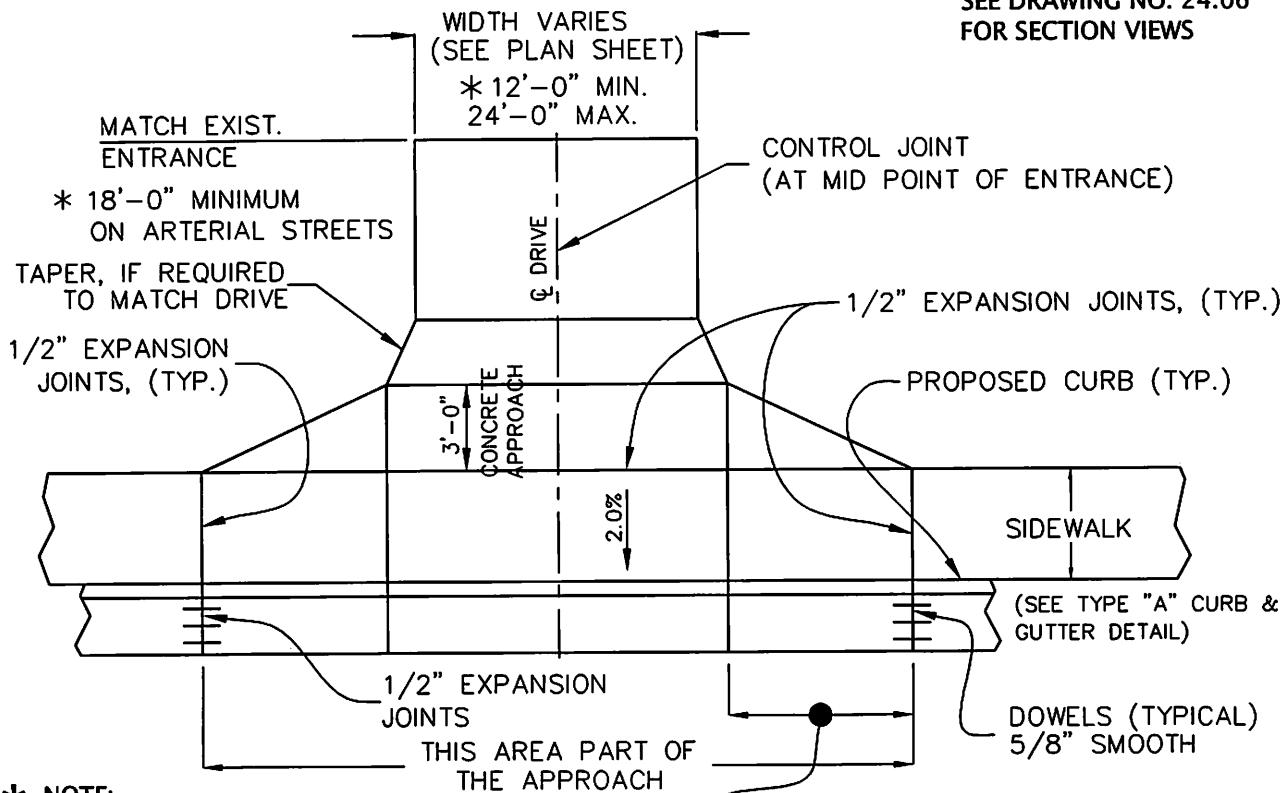
CHECKED BY:	DRAWN BY:	DATE:

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City of **Jefferson**
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION

SHEET NUMBER
3 OF 7
SECTION
24.03

SEE DRAWING NO. 24.06
FOR SECTION VIEWS

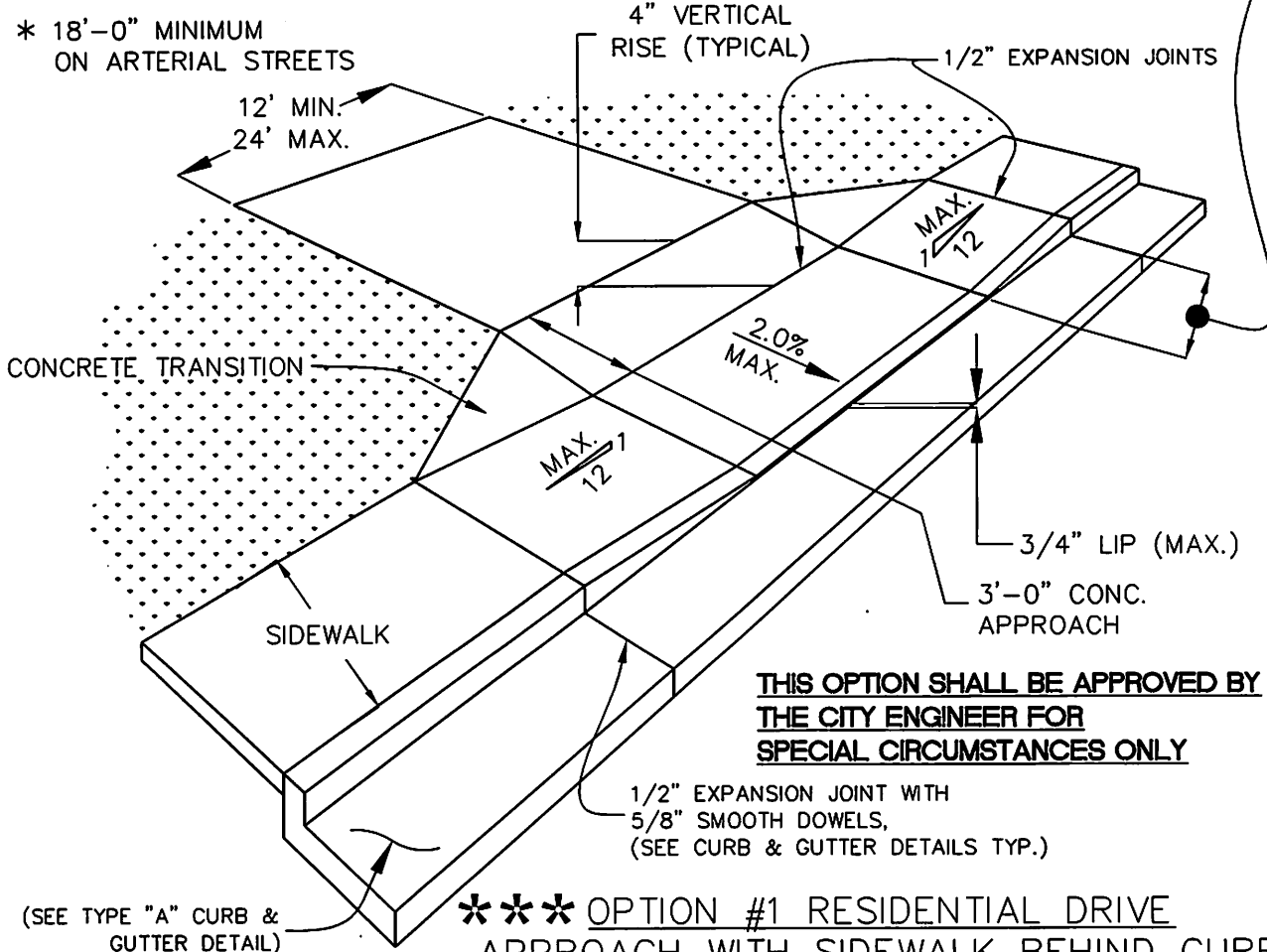


* NOTE:
RAMPS ON LOWER SIDE OF DRIVE APPROACH
OR CROSS STREET SHOULD SLOPE TOWARD
THE DRIVE APPROACH OR CROSS STREET
TO ACHIEVE POSITIVE DRAINAGE.

* RAMP GRADE SHALL NOT EXCEED 12:1
SLOPE, UP TO A RAMP LENGTH OF
15 FEET. NO RAMP STEEPER THAN 12:1
SHALL BE LESS THAN 15 FEET.

PLAN VIEW

* 18'-0" MINIMUM
ON ARTERIAL STREETS



*** OPTION #1 RESIDENTIAL DRIVE
APPROACH WITH SIDEWALK BEHIND CURB

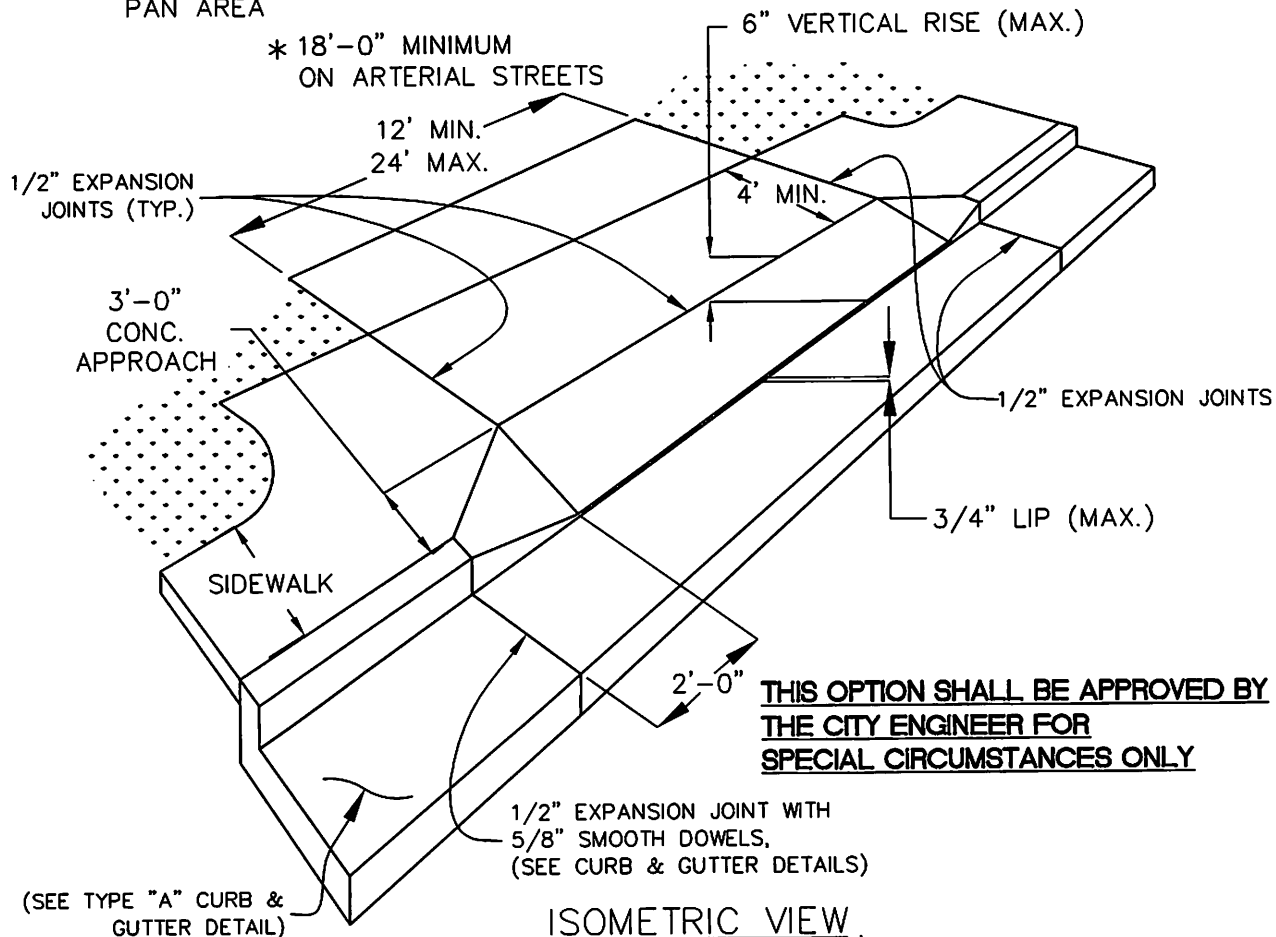
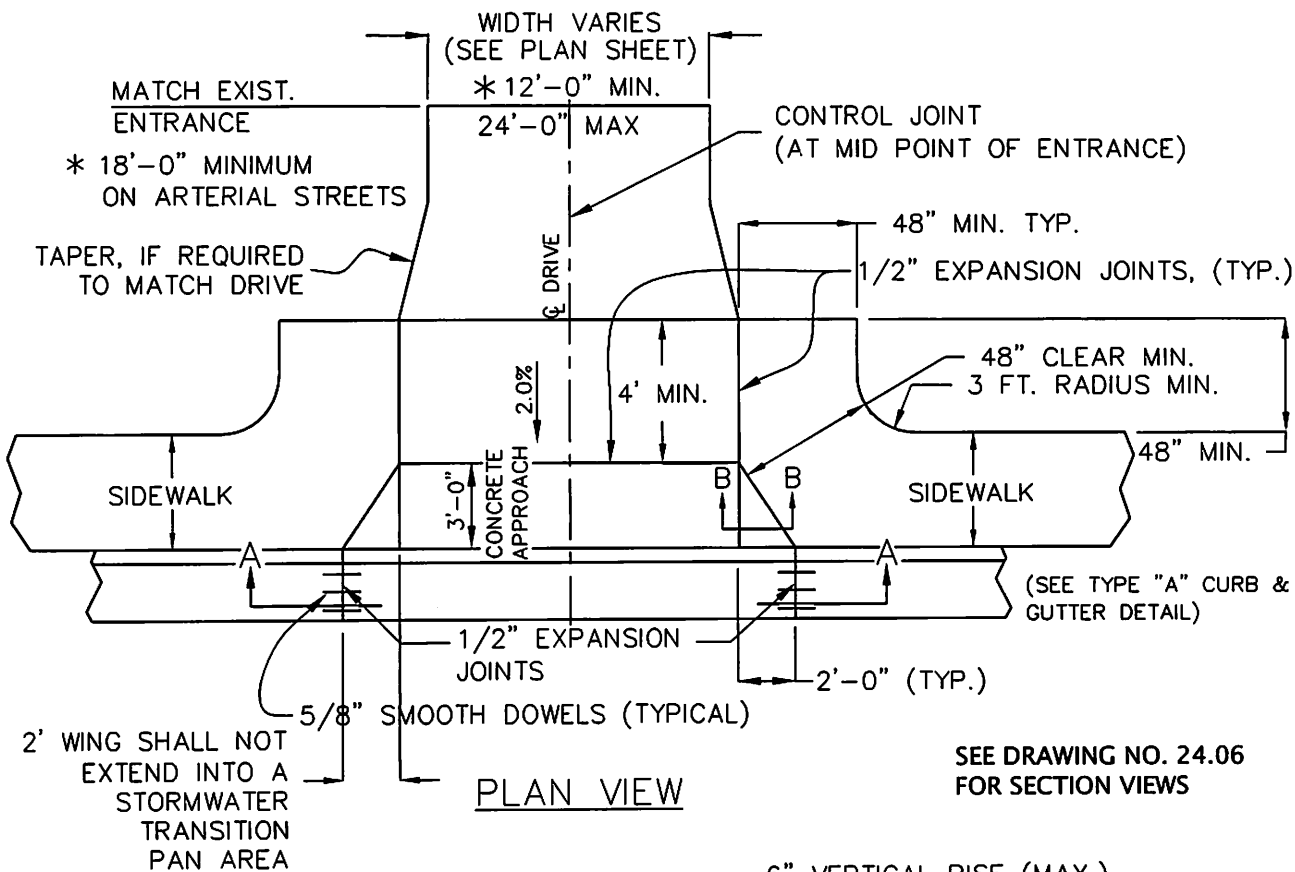
NO.	DATE	REVISION AND DESCRIPTION
1	1/2010	*** UPDATED FOR OPTIONAL SIDEWALK LAYOUT
2	1/2013	* NOTE: A.D.A. RAMP GRADES/LENGTHS

RESIDENTIAL DRIVE
APPROACH DETAILS
LOCATIONS IN WHICH
EXISTING SIDEWALK
IS BEHIND CURB

CHECKED BY:	DRAWN BY:	DATE:
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City of **Jefferson**
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION

SHEET NUMBER
4 OF 7
SECTION
24.04



OPTION #2 RESIDENTIAL DRIVE
APPROACH WITH SIDEWALK BEHIND CURB

24.01.DWG

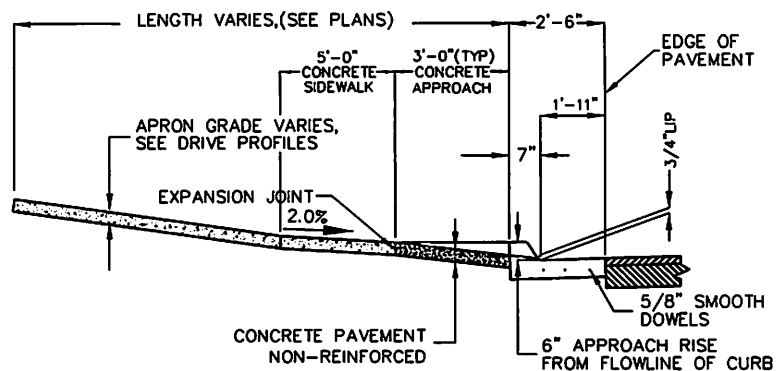
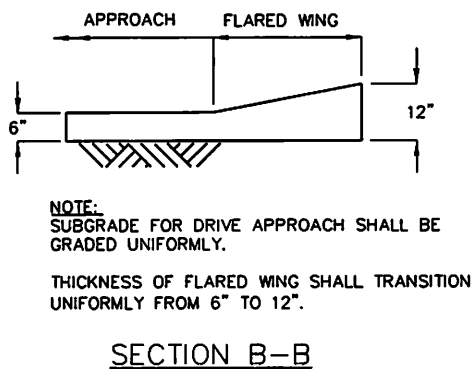
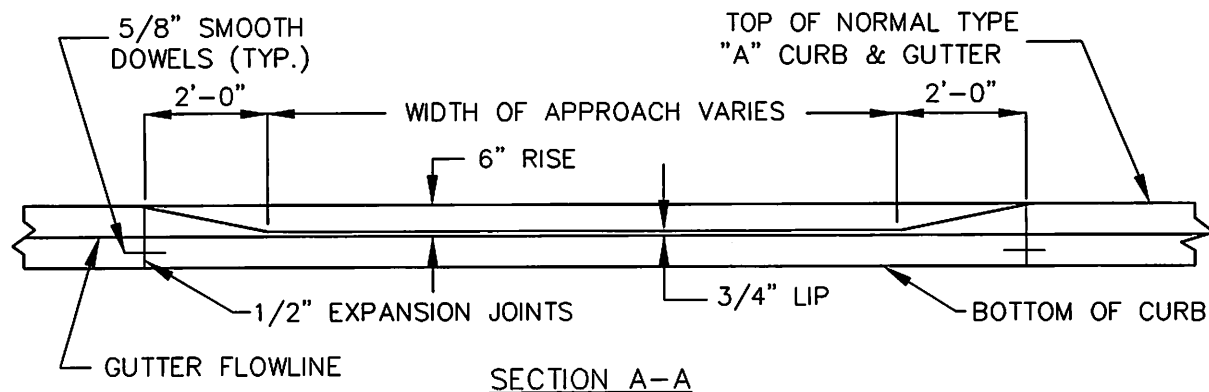
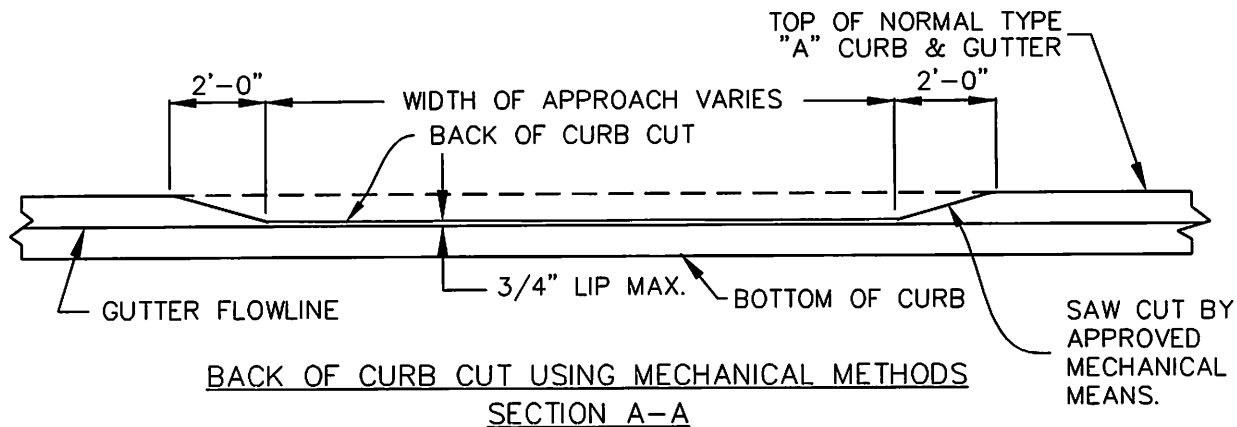
NO.	DATE	REVISION AND DESCRIPTION
1	1/2010	*** UPDATED FOR OPTIONAL SIDEWALK LAYOUT

RESIDENTIAL DRIVE
APPROACH DETAILS
LOCATIONS IN WHICH
EXISTING SIDEWALK
IS BEHIND CURB

CHECKED BY:	DRAWN BY:	DATE:
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SHEET NUMBER
5 OF 7
SECTION
24.05



TYPICAL SECTION OF RESIDENTIAL ENTRANCE

REVISION AND DESCRIPTION

DATE:

NO:

RESIDENTIAL DRIVE
SECTIONS

CHECKED BY:

DRAWN BY:

DATE:

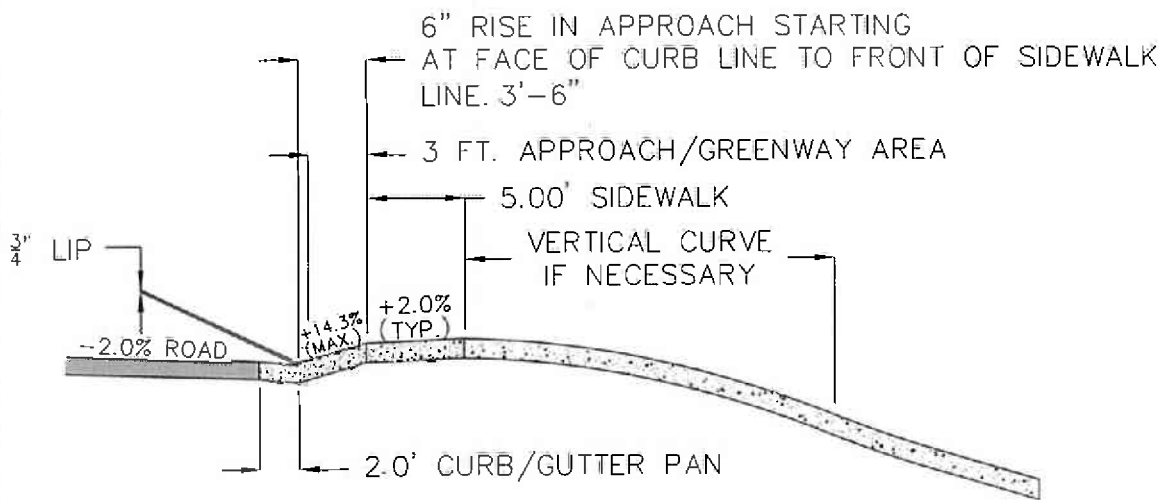
City of **Jefferson**
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION

SHEET NUMBER

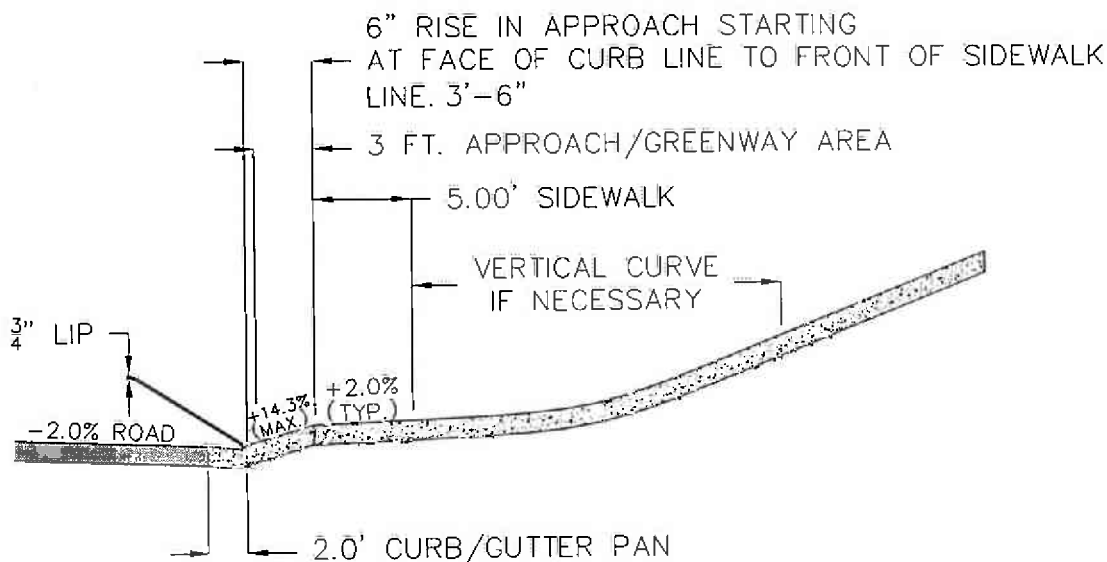
6 OF 7

SECTION

24.06



**STANDARD DRIVEWAY DOWNGRADE PROFILE LAYOUT
WITH 3'-0" GREENWAY AND 5'-0" SIDEWALK**



**STANDARD DRIVEWAY UPGRADE PROFILE LAYOUT
WITH 3'-0" GREENWAY AND 5'-0" SIDEWALK**

NOTES:
THE CONTRACTOR SHALL BUILD THE DRIVEWAY SO THAT VEHICLES
WILL NOT "BOTTOM OUT" OR "DRAG" WHILE DRIVING UP/DOWN THE DRIVEWAY.

REVISION AND DESCRIPTION

DATE:

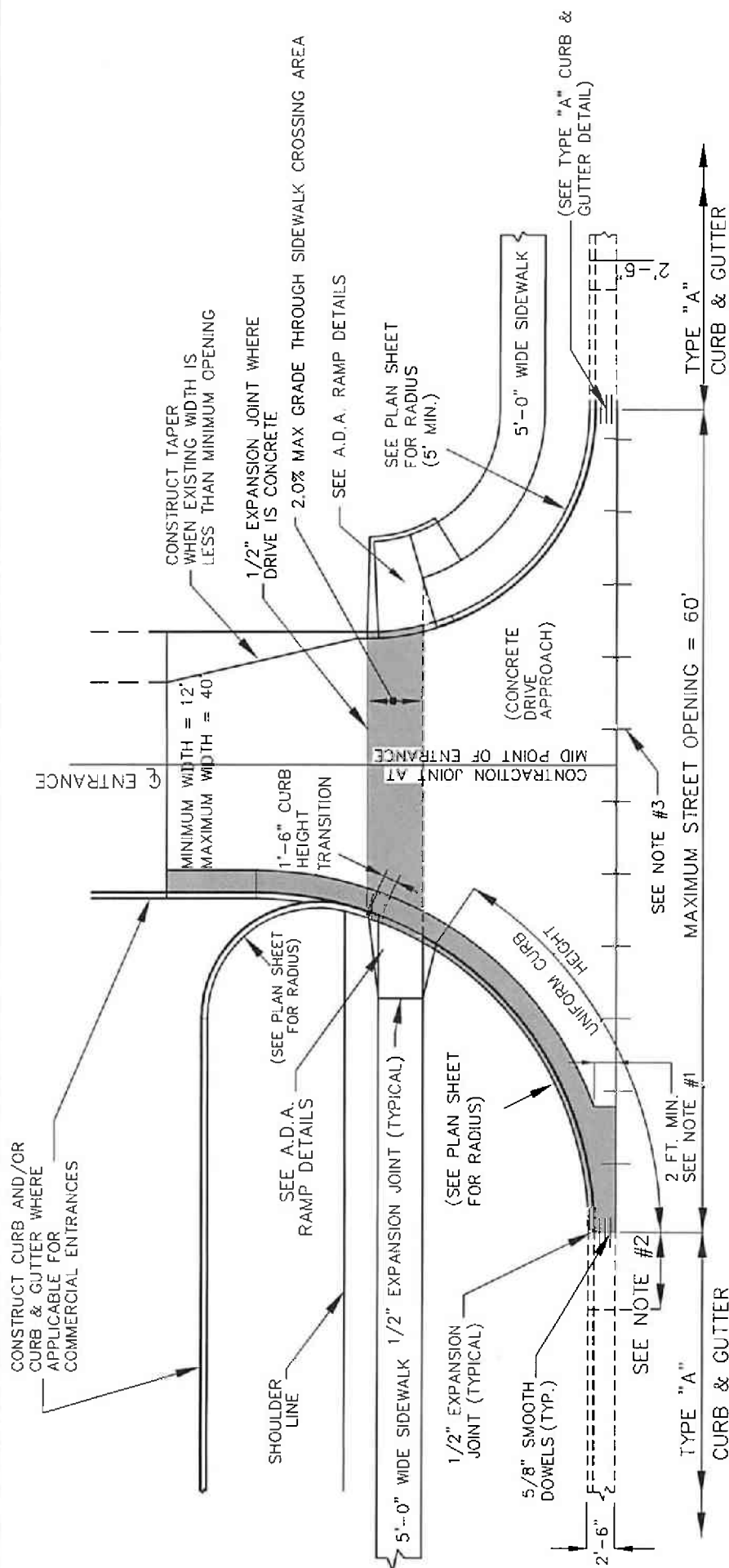
NO:

STANDARD DRIVEWAY
PROFILES FOR
MAXIMUM
DOWNGRADE/UPGRADE
VERTICAL CURVES

CHECKED BY:

DRAWN BY:

DATE:



NOTES:

1. IF CONCRETE APPROACH IS NOT POURED MONOLITHICALLY, AND THE CURB AND GUTTER IS POURED SEPARATELY IN THE APPROACH AREA, THE CURB AND GUTTER SHALL BE CONSTRUCTED AS SHOWN WITH A 2 FT. MINIMUM DISTANCE FROM THE FACE OF THE APPROACH.
2. REPLACEMENT OF CURB AND GUTTER AND SIDEWALK IS REQUIRED IF EXISTING CONTROL JOINTS WITHIN 2 FT. OF APPROACH EXPANSION JOINT, (TYPICAL), (ASPHALT ROADWAY).
3. #4 DEFORMED DOWEL BARS 24" LONG AT 30" SPACINGS. OMIT IF APPROACH IS ABUTTING ASPHALT ROADWAY.

SEE PAGE 20.06 FOR CONCRETE REINFORCEMENT REQUIREMENTS



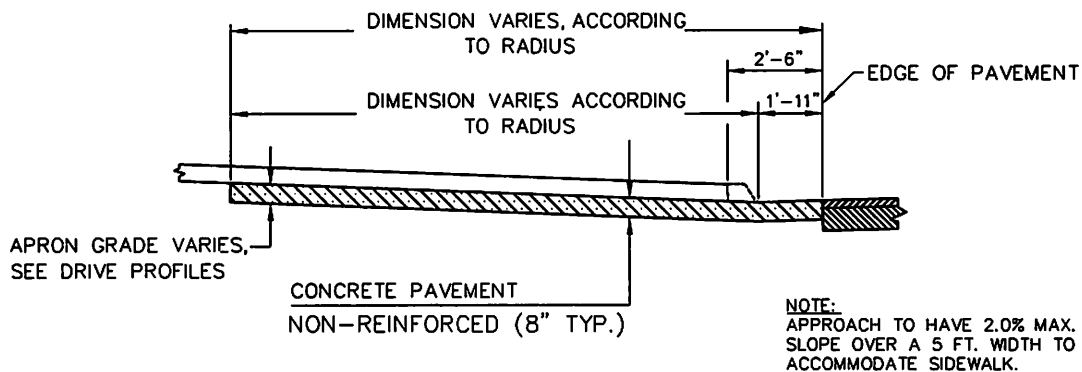
SHEET NUMBER
2 OF 3
SECTION
25.02

CHECKED BY:
DRAWN BY:
DATE:

COMMERCIAL DRIVE
APPROACH DETAILS

STANDARD DETAILS

NO:	DATE:	REVISION AND DESCRIPTION
1	12/2006	MODIFIED A.D.A. RAMP LAYOUT



TYPICAL SECTION OF COMMERCIAL ENTRANCE

<div>City of Jefferson DEPARTMENT OF PUBLIC WORKS ENGINEERING DIVISION</div>				COMMERCIAL DRIVE SECTION		NO:		DATE:		REVISION AND DESCRIPTION	
						1		4/2007		CHANGED COMMERCIAL CONCRETE DRIVE THICKNESS FROM 6" TO 8"	
				CHECKED BY:		DRAWN BY:		DATE:			
<div>SHEET NUMBER 3 OF 3 SECTION 25.03</div>											